



# Planning Justification Report

## **PREPARED FOR:**

**Official Plan and Zoning By-Law Amendment  
H. Bye Construction  
255 Watson Drive, Mount Forest**

File no. 24367A

**June 2025**

**Updated September 2025**



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# 1.0 Introduction

MHBC Planning has been retained by H. Bye Construction ("the owner" – also referred to as Randharr) to assist with the submission of an Official Plan and Zoning By-law Amendment to be permit the development of a portion of their lands municipally known as 255 Watson Drive (previously known as 275 Coral-lea Drive), Mount Forest with industrial uses. The lands subject to this application comprise approximately 10.14 hectares within the northeastern quarter of the overall site (overall owner's lands) and are herein referred to as the 'subject lands'. The subject lands consist of two portions (see **Figure 1**) referenced throughout this report as:

- 'Yardistry lands' – 2.54-hectare western portion of the subject lands
- 'Viking-Cives expansion lands' – 7.60-hectare norther portion of the subject lands

The subject lands are immediately adjacent to the Industrial Business Park Settlement Area boundary, in the County of Grey Official Plan and are located south of Norpark Avenue, east of Harry Bye Boulevard, north of the Coral Lea Avenue Road allowance and adjacent to the terminus of Viola May Crescent. The lands are vacant and currently farmed as an interim use.

The owner intends to develop Assessment Parcel 420501000702430 to accommodate a new 3,900 m<sup>2</sup> industrial building for Viking-Cives. The intent is that through a future lot addition, the Viking Cives expansion lands would be added to Assessment Parcel 420501000702430 to accommodate a further future expansion of the proposed Viking-Cives building (3,900 sq. m expansion), and an outdoor storage/staging area for the parking of finished products awaiting delivery.

The owner also intends to sever the Yardistry lands and develop them with a new 4,645 m<sup>2</sup> manufacturing building with ancillary parking and loading spaces. The intent is that through a future lot addition, the Yardistry lands would be added to Parcel 420501000702460 to provide access and frontage onto the current terminus of Viola May Crescent.

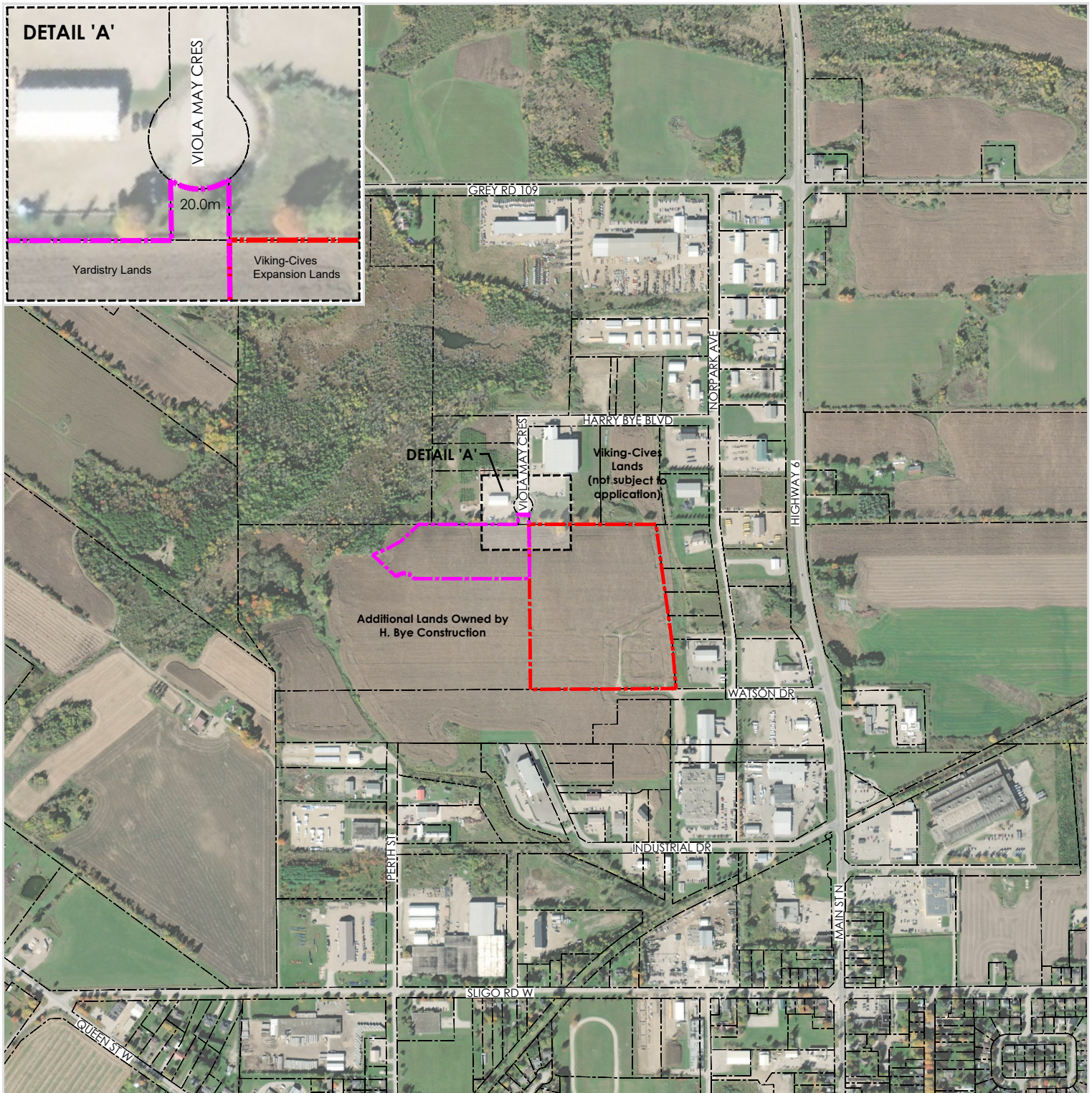
The proposed lot configuration is as follows:

- Viking Cives expansion lands (7.6 ha) to be severed and merged with Roll No. 420501000702430 (2.06 ha).
- Yardistry lands (2.54 ha) to be severed and merged with Roll No. 420501000702460 (0.04 ha).
- The remainder of Roll No. 420501000702500 is to be retained and is not subject to the proposed Official Plan and Zoning By-law amendment applications.

The Official Plan Amendment proposes to expand the Settlement Area boundary to the east, to include the subject lands (both the Viking Cives expansion lands and the Yardistry lands). The Zoning By-law Amendment proposes to zone the subject lands (both the Yardistry lands and Viking-Cives expansion lands) for industrial use. We note a portion of the lands planned for the Viking-Cives building are already zoned for the intended use.

The remainder of 255 Watson Drive is referred herein as 'additional lands owned by H. Bye Construction.' These lands are not currently part of this application. In the future, and through a





**Figure 1:  
Location Map**

**LEGEND**

- Viking-Cives Expansion Lands
- Yardistry Lands

**DATE:** February, 2025

**SCALE:** 1:10,000

**FILE:** 24367A

**DRAWN:** GC



K:\24367A- MOUNT FOREST\RPT\LOCATION.DWG

PT LT 32 CON 1 DIVISION 1 NORMANBY;  
PT LT 32 CON 1 DIVISION 2 NORMANBY  
PT 7, 8 & 9 17R2039 AND PT 1 17R2090;  
T/W GS171216; WEST GREY

Source: Bing Aerial Imagery



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separate planning approval process, the owner plans to develop the additional lands with an industrial subdivision. The proposed development of the Yardistry and Viking-Cives expansion lands is the focus of this report.

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## 1.1 Formal Consultation and Submission Requirements

A Formal Pre-Consultation meeting was held for the development of the subject lands on November 27, 2024, with County and West Grey staff. Following the meeting, a Formal Consultation Document was issued (December 4, 2024), identifying the planning processes and technical studies required to permit the proposed development of the subject lands.

The required items for the Official Plan and Zoning By-law Amendment application have been completed and included as part of this submission, as follows:

- Planning Justification Report (to address the settlement area expansion policies of the 2024 PPS and the Grey County Official Plan)
- Conceptual Site Plan
- Servicing Options Study
- Functional Servicing Report or Hydrogeologic and Nitrate Studies (if the Servicing Options Study recommends the use of individual private services)
- Stormwater Management Report
- Archaeological Assessment
- Environmental Impact Study
- Traffic Impact Study or Opinion Letter
- Documented pre-submission consultation with MTO, Wellington North and Wellington County

A brief summary of the technical reports and plans is included in Section 4.0 of this report. A copy of the Formal Consultation Document is included as Appendix A of this report.

## 2.0 Site Description and Surrounding Land Uses

The subject lands are located within Mount Forest, immediately adjacent to the 'Industrial Business Park Settlement Area' boundary, within the Municipality of West Grey (**see Figure 1**). The lands along Harry Bye Boulevard planned for development of a new Viking-Cives facility (Assessment Parcel 420501000702430) are within the Settlement Area boundary and already zoned for industrial uses.

Generally, the lands are surrounded by industrial and commercial development, with small areas of agricultural use and environmental features on the lands or to the southwest. The lands will be accessed from Harry Bye Boulevard and through the terminus of Viola May Crescent.

A context plan is included as **Figure 2: Context Map**. The following provides a review of surrounding land uses:

**NORTH:** To the north of the subject lands is Norpark Avenue, which contains other commercial and industrial uses. Further to the north is Highway 6, which traverses through Mount Forest, with larger commercial uses oriented along the Highway frontage.



**Image 1.** View north of the subject lands (location of subject lands indicated by star)



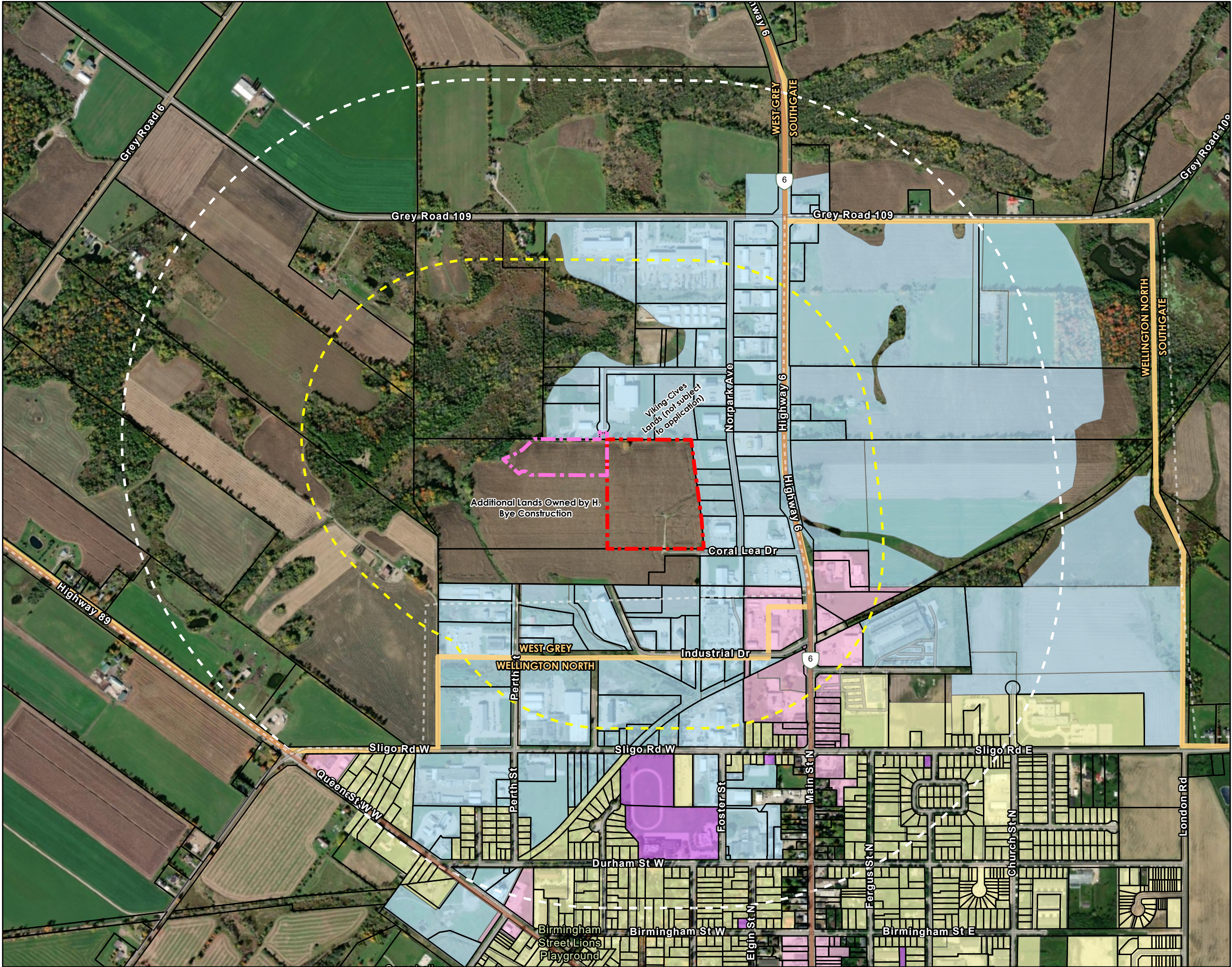


Figure 2: Context Plan

PT LT 32 CON 1 DIVISION 1 NORMANBY;  
PT LT 32 CON 1 DIVISION 2 NORMANBY  
PT 7, 8 & 9 17R2039 AND PT 1 17R2090;  
T/W GS171216; WEST GREY

LEGEND

- Viking-Cives Expansion Lands
- Yardistry Lands
- 500m Buffer
- 1km Buffer
- Municipal Boundary
- Commercial Use
- Industrial Use
- Residential
- Institutional

Source:  
• Contains information licensed under the Open Government Licence – Ontario.  
• Land uses approximate/ based on Zoning & Official Plan info.

Date:	February, 2025
File:	24367A
Scale:	1:10,000
Drawn:	GC



Document Path: K:\24367A- Mount Forest\RP\ContextPlan.aprx



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**EAST:** To the east of the subject lands are additional commercial and industrial uses, along Norpark Avenue, Coral Lea Drive and Highway 6. Further to the east is Industrial Drive, which also includes commercial and industrial uses.



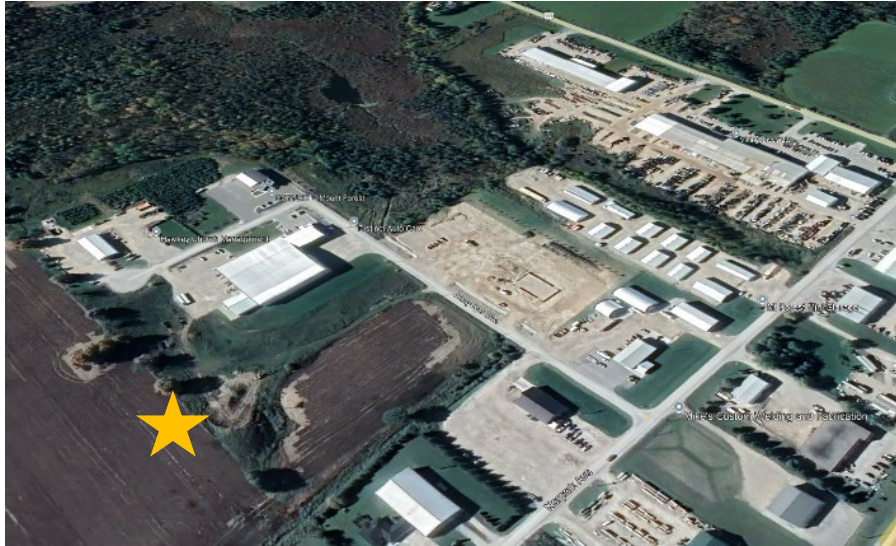
**Image 2.** View east of the subject lands.

**SOUTH:** South of the lands proposed as part of this Official Plan and Zoning By-law Amendment are other lands owned by the applicant, which are used for agricultural purposes on an interim basis. Further south are environmental features and more permanent agricultural operations. The farmed portion of the subject lands is separated from other agricultural uses to the south by a drainage feature and commercial uses along Perth Street.



**Image 3.** View looking south over the subject lands.

**WEST:** To the west of the subject lands is Harry Bye Boulevard, which contains developed and vacant commercial and industrial lands. A portion of the lands planned for this development are located on Harry Bye Boulevard, as shown in Figure 1. Viking Cives has an existing operation further to the west on County Round 71.



**Image 4.** View looking west over the subject lands

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# 3.0 Proposed Development

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## 3.1 Proposed Development

The proposed planning applications seek to bring the subject lands into the Industrial Business Park Settlement Area and permit industrial uses to facilitate the development of a new Viking-Cives building and outdoor storage area and a new industrial building on the Yardistry Lands.

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### 3.1.1 Yardistry Lands

Yardistry manufactures backyard products and outdoor shade structures. They operated within an existing manufacturing facility in Mount Forest (located at 375 Sligo Road West) specializing in gazebo manufacturing as shown in the image below:



Yardistry's current lease arrangement at their Mount Forest location is ending, necessitating their relocation. H. Bye intends to develop the 2.54 hectare portion of the subject lands, (the 'Yardistry Lands'), with a 4,645 m<sup>2</sup> manufacturing building with ancillary parking and loading spaces to accommodate Yardistry. The Yardistry Lands are proposed to be merged with Roll No. 420501000702460 (0.04ha) to provide access from and frontage on Viola May Crescent. The development is intended to be individually serviced via private well and septic system. A concept plan of the Yardistry development is included as **Figure 3**.



The Yardistry workshop will construct, package, store, and deliver outdoor structures, with several offices for administrative and design staff. The proposed workshop has a planned maximum of 40 employees and consists of manufacturing space and ancillary spaces (including office space), and 4 truck loading bays.

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### 3.1.2 Viking-Cives Lands

Viking-Cives Ltd. has been producing snow and ice control equipment out of their Mount Forest manufacturing facility since 1985. Due to increasing demand for their products, Viking Cives needs to expand their facility. Given the proximity of the subject lands to the existing Viking-Cives facility, the company is pursuing purchase of Assessment Parcel 420501000702430 to build a new manufacturing building. However, the lot is not large enough to accommodate all of their needs. They require an expansion (through a future lot addition) to include an additional 7.60-hectares (Viking-Cives expansion lands) from 255 Watson Drive to Assessment Parcel 420501000702430.

Viking-Cives intends to utilize the new site for product storage in the short term and to build manufacturing installation bays in the coming 2-4 years. The future build out of the site would include a new 3900 m<sup>2</sup> manufacturing building (with additional space to accommodate a future 3900 m<sup>2</sup> expansion), and 448 truck body staging spots where finished products are parked until they are shipped or delivery is taken.

The facility would produce equipment for snow plowing and attach the plow equipment to trucks (which are brought to the facility), like the image below:



The products would then be driven out of the manufacturing building and parked in organized rows on the lands, awaiting shipment/delivery to their end users.

An internal driveway connects through to Harry Bye Boulevard which will be the main access into and out of the site, until Viola May Crescent is extended through a future and separate planning approval processes. A potential minor extension of Coral Lea Drive would also provide a third access to the lands,

though that extension is not required. The development is intended to be individually serviced via private well and septic system.

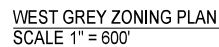
As discussed, a portion of the lands planned for development is already located within the Settlement Area boundary and zoned for industrial uses. Viking-Cives requires a larger parcel both for the manufacturing facility and the storage and staging area of the finished products. In order to accommodate their development plans, an expansion to the Settlement Area boundary is required.

---

### 3.1.3 Development Applications

An Official Plan Amendment is proposed to expand the Settlement Area boundary and to designate the subject lands (both the Yardistry and Viking-Cives Extension Lands) 'Industrial Business Park Settlement Area'. Similarly, a Zoning By-law Amendment is also proposed to zone the subject lands 'Industrial Zone' with site specific exception 237 (M1-237) to permit the proposed industrial manufacturing use.

### Figure 3 - Consent Sketch



PARCEL DESCRIPTION	
ARN	#2050100070102450
PRIMARY ADDRESS	
MUNICIPALITY	Municipality of West Gt
LEGAL DESCRIPTION	NORMANBY CON:1 PT QIV 3 LOT:31 PR:16R5454 PART 1
PROPERTY USE	Vacant industrial land
YES	Outside Niagara Escarpment Plan Area
FRONTAGE (m)	---
DEPTH (m)	---
AREA (ac)	3.59 ac
SEWER TYPE	Private Service
WATER TYPE	Private Service
HYDRO ACCESS	YES
PROPERTY ACCESS	YES

SECTION 24 - M1-237 - INDUSTRIAL ZONE (MUNICIPALITY OF WEST GREY ZONING BY-LAW 37-2006)			
	DESCRIPTION	MINIMUM/MAXIMUM ZONING REQUIREMENTS	PROVIDED
24.1	PERMITTED USES	Any manufacturing, processing, assembly, repair, fabricating, milling except for a motor vehicle recycling and salvage or wrecking facility, junk or scrap yard, fertilizer manufacturer, abator, rendering plant or any use considered offensive by the Public Health Act	COMPLIES

ENGINEER:

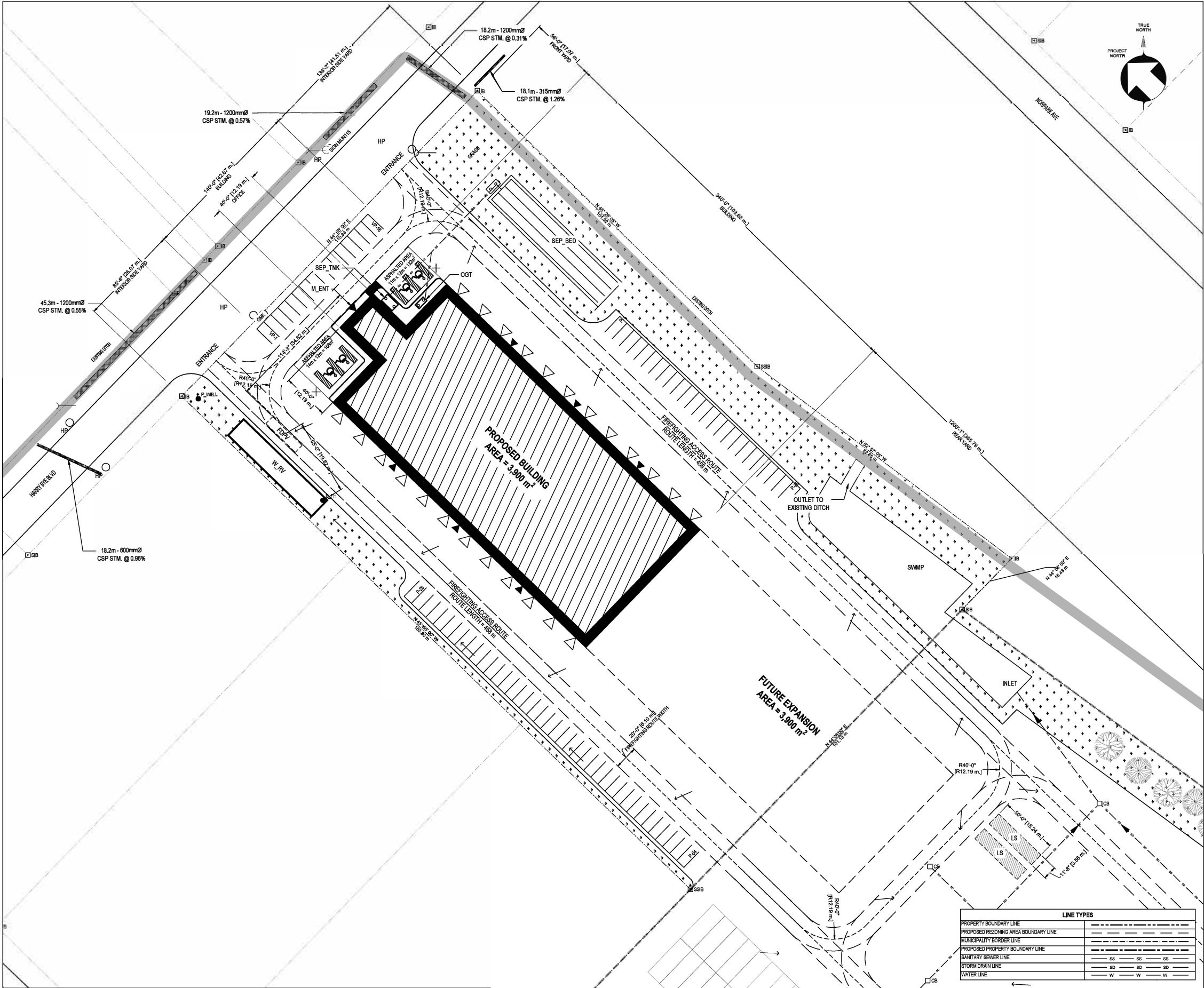
**H. BYE**  
**CONSTRUCTION**  
LIMITED  
MOUNT FOREST  
**323-1520**  
*Since 1954*

SHEET TITLE:  
LOT FABRIC AND REZONING PLAN

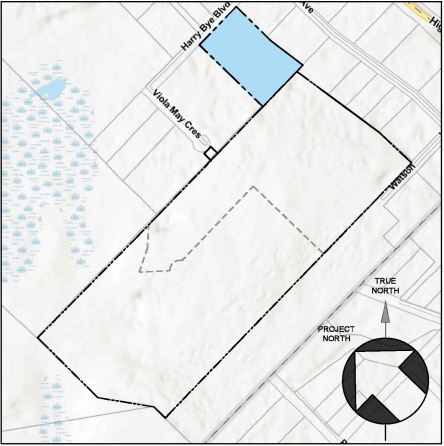
DWG FILE:	--		
DRAWN BY:	R.M.M.	JOB#	2025.0076
CHECKED BY:	H.B.	A1	
REVISION:	N/A		
DATE:	APRIL 23, 2025		
SCALE:	AS NOTED		







VIKING CIVES SITE PLAN  
SCALE 1"=40'



KEY PLAN  
SCALE 1" = 600'

LEGEND			
DESCRIPTION	TAG	DESCRIPTION	TAG
OVERHEAD DOOR	▽	WATER RESERVOIR	W_RV
EGRESS DOOR	▽	FIRE DEPARTMENT PUMPER VEHICLE	FDPV
IRON BAR	■	GRASS	---
SHORT STANDARD IRON BAR	■	MAIN ENTRANCE	M_ENT
STANDARD IRON BAR	■	PARKING SPOT	P-#
CATCH BASIN	□	VISITOR'S PARKING SPOT	VP-#
MANHOLE	□	BARRIER FREE PARKING SPOT	BP-#
PROPOSED FIRE HYDRANT	●	SEPTIC TANK	SEP_TNK
PROPOSED WELL	●	ISLAND GREASE TRAP	OGT
STORM POND	SWMP	SEPTIC BED	SEP_BED
HYDROPOLE	○	SIGN	—
LOADING SPACE	LS		

SECTION 24 - M1-237 - INDUSTRIAL ZONE (MUNICIPALITY OF WEST GREY ZONING BY-LAW 37-2006)		
DESCRIPTION	MINIMUM/MAXIMUM ZONING REQUIREMENTS	PROVIDED
24.1 PERMITTED USES	Any manufacturing, processing, assembly, repair, fabricating, milling except for a motor vehicle recycling and salvage or wrecking facility, junk or scrap yard, fertilizer manufacturer, abattoir, rendering plant or any use considered offensive by the Public Health Act	COMPLIES
M1-237 LOT AREA, MINIMUM	0.8 ha	9.98 ha
24.2.2 LOT FRONTAGE, MINIMUM	30 m (98.4 ft)	110.34 m
M1-237 FRONT YARD, MINIMUM	15 metres	17.07 m
24.2.4 INTERIOR SIDE YARD, MINIMUM	3 m (9.8 ft), 9.2 m (30.2 ft) where an M1 Zone abuts any residential zone.	EAST 41.51 m WEST 28.07 m
24.2.5 EXTERIOR SIDE YARD, MINIMUM	7.5 m (24.6 ft)	NA
24.2.6 REAR YARD, MINIMUM	7.5 m (24.6 ft), 9.2 m (30.2 ft) where an M1 Zone abuts any residential zone.	365.78 m
24.2.7 LOT COVERAGE, MAXIMUM	60%	4.04% (3,900 m <sup>2</sup> )
M1-237 BUILDING HEIGHT, MAXIMUM	15 metres	9.5 m
24.3 PARKING SPACE REGULATION - VISITOR	In addition to the required number of parking spaces set out in Section 6.27, a maximum of three visitor parking spaces for passenger vehicles may be provided in the required front yard for the first 15 m (49.2 ft) of front wall of the principal building plus one additional visitor parking space for each additional 7.5 m (24.6 ft) of front wall in excess of the first 15 m (49.2 ft).	18 VISITOR PARKING SPACES
24.3.7 PARKING REGULATIONS	6.27.1 Sized Parking Spaces 6.27.2 Access to Parking Spaces 6.27.3 Parking Surfaces 6.27.4 Location of Parking Areas and Spaces 6.27.5 Barrier Free Parking 6.27.6 Calculation of Parking Regulations	COMPLIES COMPLIES COMPLIES COMPLIES COMPLIES COMPLIES
24.4 ACCESSORY RETAIL USE	Accessory retailing of products shall be permitted subject to the following regulations: a) A maximum of 25% of the gross floor area is used within i) The main industrial building, or ii) Each individual unit in an industrial mall. b) The products to be sold must be produced on the site. A Buffer Area/Stripes shall be required where a M1 Zone directly abuts any Residential Zone subject to the applicable regulations of Section 6.3 of this By-law.	NA
24.5 (a) OTHER PROVISIONS - a)	A Buffer Area/Stripes shall be required where a M1 Zone directly abuts any Residential Zone subject to the applicable regulations of Section 6.3 of this By-law.	NA
M1-237 MINIMUM LANDSCAPED OPEN SPACE	10%	10.01% (9,96 ha)
24.5 (b) OTHER PROVISIONS - b)	Loading requirements shall be in accordance with the applicable regulations of Section 6.16 of this By-law.	2 LOADING SPACES
24.5 (c) OTHER PROVISIONS - c)	Outdoor storage areas shall be in accordance with the applicable regulations of Section 6.28 of this By-law.	COMPLIES
24.5 (d) OTHER PROVISIONS - d)	Garbage Storage Areas shall be provided in accordance with the applicable regulations of Section 6.11 of this By-law.	COMPLIES

- 3.2.5.6. LOCATION OF ACCESS ROUTES [IBC 2024]**
- Access routes for firefighting shall be located so that the principal entrance is located not less than 3 m (9'-10") and not more than 15 m (49'-3") from the closest portion of the access route required for fire department use, measured horizontally from the face of the building.
  - Access routes shall be provided to a building so that:
    - for a building provided with a fire department connection, a fire department pumper vehicle can be located adjacent to the hydrants referred to in Article 3.2.5.16, 2.b.
    - for a building not provided with a fire department connection, a fire department pumper vehicle can be located so that the length of the access route from a hydrant to the vehicle plus the unobstructed path of travel for the firefighter from the vehicle to the building is not more than 90 m (295'-3"), and the unobstructed path of travel for the firefighter from the vehicle to the building is not more than 45 m (147'-8").
  - The unobstructed path of travel for the firefighter required by Sentence (2) from the vehicle to the building shall be measured from the vehicle to the fire department connection provided for the building, except that if no fire department connection is provided, the path of travel shall be measured to the principal entrance of the building.
  - If a portion of a building is completely cut off from the remainder of the building so that there is no access to the remainder of the building, the access routes required by Sentence (2) shall be located so that the unobstructed path of travel from the vehicle to one entrance of each portion of the building is not more than 45 m (147'-8").
- 3.2.5.6 ACCESS ROUTE DESIGN [IBC 2024]**
- A portion of a roadway or yard provided as a required access route for fire department use shall:
    - have a clear width not less than 6 m, unless it can be shown that lesser widths are satisfactory,
    - have a centreline radius not less than 12 m,
    - have an overhead clearance not less than 5 m,
    - have a change of gradient not more than 1 in 12.5 over a minimum distance of 15 m,
    - be designed to support the expected loads imposed by firefighting equipment and be surfaced with concrete, asphalt or other material designed to permit accessibility under all climatic conditions,
    - have turnaround facilities for any dead-end portion of the access route more than 90 m long, and
    - be connected with a public thoroughfare.

**6.9 EXTERNAL LIGHTING [MUNICIPALITY OF WEST GREY ZONING BY-LAW 37-2006]**  
Lighting fixtures providing exterior illumination shall be so arranged as to direct light to the building face, parking area or vicinity of site. Such fixtures shall not cast glare onto adjacent properties nor onto nearby streets.

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Figure 4 -  
Viking Cives  
Concept Plan

DRAWING REVISIONS			
No.	DATE	REVISION	BY

DRAWING RELEASE			
DATE	ISSUE	BY	APPD.

ENGINEER:

**H. BYE CONSTRUCTION LIMITED**  
MOUNT FOREST  
323-1520  
Since 1954

OWNER:

CONSTRUCTION BY:

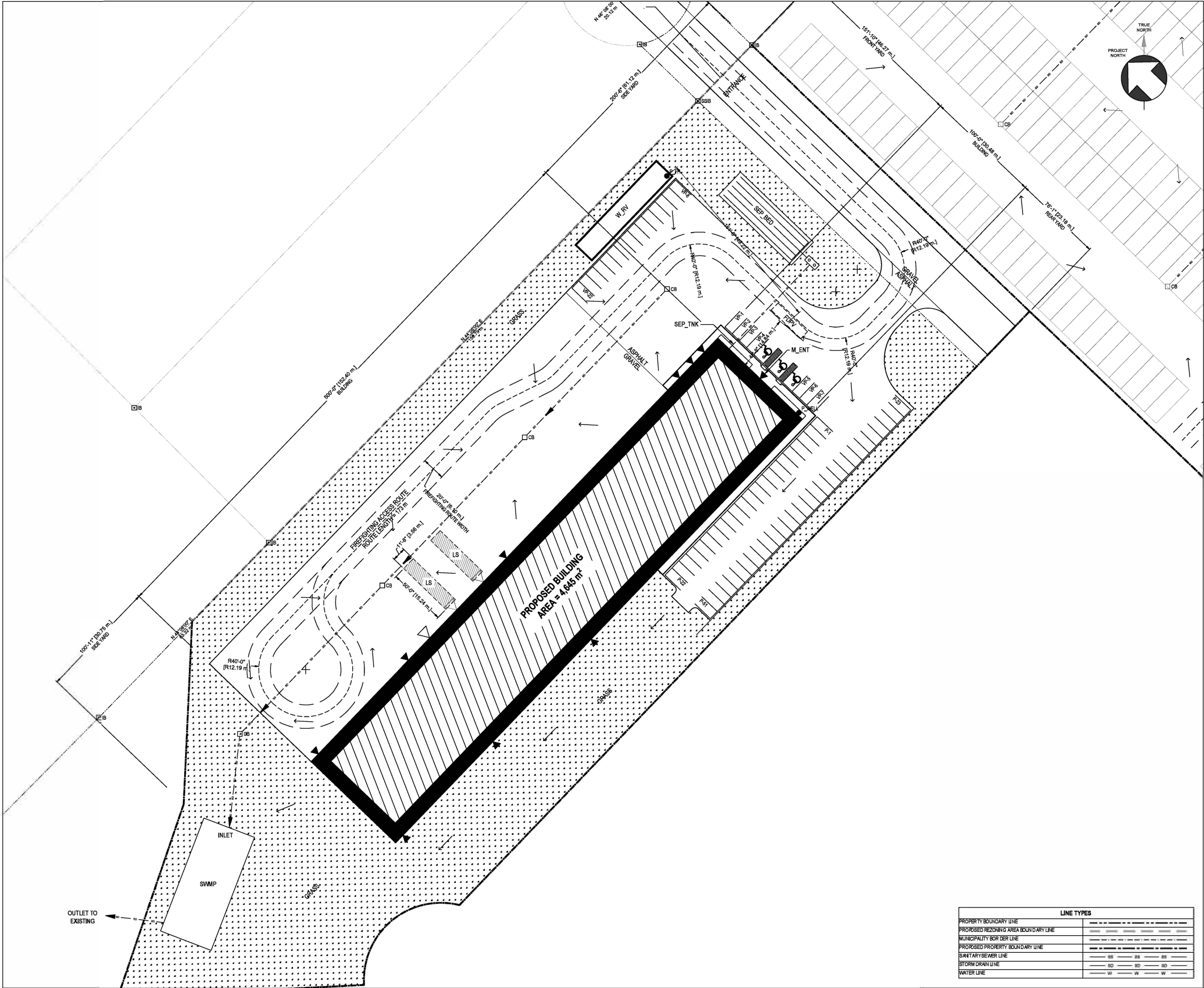
PROJECT NAME LOCATION:

SHEET TITLE:  
VIKING CIVES SITE PLAN

DWG FILE	-	DATE	2025.03.19
DRAWN BY	R.M.	JOH	
CHECKED BY	H.B.		
REVISION	NA		
DATE	APRIL 28 2025		
SCALE	AS NOTED		

A4





YARDISTRY SITE PLAN  
SCALE 1"=40'



KEY PLAN  
SCALE 1" = 600'

LEGEND			
DESCRIPTION	TAG	DESCRIPTION	TAG
OVERHEAD DOOR	▽	WATER RESERVE VEHICLE	W_V
EGRESS DOOR	▽	FIRE DEPARTMENT PUMPER VEHICLE	FDV
IRON BAR	■	GRASS	GR
SHORT STANDARD IRON BAR	■	MAIN ENTRANCE	M_ENT
STANDARD IRON BAR	■	PARKING SPOT	P#
CAT CH BASIN	□	VISITOR'S PARKING SPOT	V#
MAN HOLE	□	BARRIER FREE PARKING SPOT	BF
PROPOSED FIRE HYDRANT	●	SEPTIC TANK	SEP_TNK
PROPOSED WELL	●	OIL AND GREASE TRAP	OGT
STORM POND	○	SEPTIC BED	SEP_BED
HYDRO POLE	○	SIGN	▲
LOADING SPACE	LS		

SECTION 24 - M1-237 - INDUSTRIAL ZONE (MUNICIPALITY OF WEST GREY ZONING BY-LAW 37-2006)		
DESCRIPTION	MINIMUM/MAXIMUM ZONING REQUIREMENTS	PROVIDED
24.1 PERMITTED USES	Any manufacturing, processing, assembly, repair, fabricating, milling, except for a motor vehicle recycling and salvage or wrecking facility, junk or scrap yard, fertilizer manufacturer, abattoir, rendering plant or any use considered offensive by the Public Health Act	COMPLIES
M1-237 LOT AREA MINIMUM	0.5 ha	2.53 ha
24.2.2 LOT FRONTAGE MINIMUM	20 m (65.6 ft)	20.12 m
M1-237 FRONT YARD MINIMUM	15 metres	46.27 m
24.2.4 INTERIOR SIDE YARD MINIMUM	3m (9.8 ft); 9.2 m (30.2 ft) where an M1 Zone abuts any residential zone	WEST 30.75 m EAST 61.12 m
24.2.5 EXTERIOR SIDE YARD MINIMUM	7.5 m (24.6 ft)	N/A
24.2.6 REAR YARD MINIMUM	7.5 m (24.6 ft); 9.2 m (30.2 ft) where an M1 Zone abuts any residential zone	23.18 m
24.2.7 LOT COVERAGE, MAXIMUM	80%	18.36% (4,845 m²)
M1-237 BUILDING HEIGHT, MAXIMUM	15 metres	9.2 m
24.3 PARKING SPACE REGULATION - VISITOR	In addition to the required number of parking spaces set out in Section 6.2.7, a maximum of three visitor parking spaces for passenger vehicles may be provided in the required front yard for the first 15 m (49.2 ft) of front wall of the principal building plus one additional visitor parking space for each additional 7.5 m (24.6 ft) of front wall in excess of the first 15 m (49.2 ft).	22 VISITOR PARKING SPACES
6.7 PARKING REGULATIONS	6.27.1 Size of Parking Spaces 6.27.2 Access to Parking Spaces 6.27.3 Parking Surface 6.27.4 Location of Parking Areas and Spaces 6.27.5 Barrier Free Parking 6.27.6 Calculation of Parking Regulations	COMPLIES COMPLIES COMPLIES COMPLIES 3 SPACES 51 SPACES
24.4 ACCESSORY RETAIL USE	Accessory retailing of products shall be permitted subject to the following regulations: a) A maximum of 25% of the gross floor area is used within: i) The main industrial building, or ii) Each individual unit in an industrial mall. b) The products to be sold must be produced on the site. c) A buffer area shall be provided between the industrial building and the accessory retailing area. d) The accessory retailing area shall be directly adjacent to the industrial building.	N/A
24.5 (a) OTHER PROVISIONS - a)	A buffer area shall be provided between the industrial building and the accessory retailing area.	N/A
M1-237 MINIMUM LANDSCAPED OPEN SPACE	10%	47.13% (11,924 m²)
24.5 (b) OTHER PROVISIONS - b)	Loading requirements shall be in accordance with the applicable regulations of Section 6.18 of this By-law.	2 LOADING SPACES
24.5 (c) OTHER PROVISIONS - c)	Outdoor storage areas shall be in accordance with the applicable regulations of Section 6.2 of this By-law.	COMPLIES
24.5 (d) OTHER PROVISIONS - d)	Garbage storage areas shall be provided in accordance with the applicable regulations of Section 6.11 of this By-law.	COMPLIES

**3.2.5.6 LOCATION OF ACCESS ROUTES**

1. Access routes for firefighting shall be located so that the principal entrance is located not less than 3 m (9'-10") and not more than 5 m (49'-3") from the closest portion of the access route required for fire department use, measured horizontally from the face of the building.

2. Access routes shall be provided to a building so that:

2.1. for a building provided with a fire department connection, a fire department pumper vehicle can be located adjacent to the hydrant as referred to in Article 3.2.5.16, 2.b.

2.2. for a building not provided with a fire department connection, a fire department pumper vehicle can be located so that the length of the access route from a hydrant to the vehicle plus the unobstructed path of travel for the firefighter from the vehicle to the building is not more than 90 m (295'-3"), and the unobstructed path of travel for the firefighter from the vehicle to the building is not more than 45 m (147'-8").

3. The unobstructed path of travel for the firefighter required by Sentence (2) from the vehicle to the building shall be measured from the vehicle to the fire department connection provided for the building, except that if no fire department connection is provided, the path of travel shall be measured to the principal entrance of the building.

4. If a portion of a building is completely cut off from the remainder of the building so that there is no access to the remainder of the building, the access routes required by Sentence (2) shall be located so that the unobstructed path of travel from the vehicle to one entrance of each portion of the building is not more than 45 m (147'-8").

**3.2.5.6 ACCESS ROUTE DESIGN**

1. A portion of a roadway or yard provided as a required access route for fire department use shall:

1.1. have a clear width of not less than 5 m, unless it can be shown that lesser widths are satisfactory;

1.2. have a centreline radius not less than 12 m;

1.3. have an overhead clearance not less than 5 m;

1.4. have a change of gradient not more than 1 in 12.5 over a minimum distance of 15 m;

1.5. be designed to support the expected loads imposed by firefighting equipment and be surfaced with concrete, asphalt or other material designed to permit accessibility under all climatic conditions;

1.6. have turnaround facilities for any dead-end portion of the access route more than 90 m long; and

1.7. be connected with a public thoroughfare.

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Figure 5 -  
Yardistry  
Concept Plan

DRAWING REVISIONS			
No.	DATE	REVISION	BY / APPD.
DRAWING RELEASE			
DATE	ISSUE	BY	APPD.

ENGINEER:

**H. BYE CONSTRUCTION**  
LIMITED  
MOUNT FOREST  
323-1520  
Since 1954

OWNER:

CONSTRUCTION BY:

PROJECT NAME / LOCATION:

SHEET TITLE:  
YARDISTRY SITE PLAN

DWG FILE	-	JOB#	205.005
DRAWN BY:	R.M.M.	CHECKED BY:	H.B.
REVISION:	NA	DATE:	APRIL 23 2025
SCALE:	AS NOTED		

A5

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## 4.0 Summary of Supporting Studies

The following reports were requested through Formal Consultation to support the request for an Official Plan and Zoning By-law Amendment application by Grey County, the Municipality of West Grey and other agencies. A brief summary of the conclusions of each report is provided below.

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### 4.1 Environmental Impact Study

SAAR Environmental Limited Biologists (SAAR) prepared an Environmental Impact Study (EIS) in support of the proposed application, dated May 2025. SAAR consulted with Saugeen Valley Conservation Authority (SVCA) and Grey County's ecologists with respect to natural heritage, hazard lands, and scoping the study.

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#### 4.1.1 Viking Cives

The Viking Cives expansion lands were previously disturbed and intensively farmed. SAAR confirmed that the level of agrarian use does not provide for rare grassland nesting birds nor the Yellow-banded Bumble Bee.

A drainage ditch is located just outside the northeastern boundary of the site. Through consultation, SAAR confirmed that SVCA staff consider the ditch an urban drainage feature not subject to SVCA Regulation. SAAR found that the disturbed condition of the roadside drain including silt load does not offer optimal habitat for herptiles.

Recommended mitigation measures include a development setback of 10-metres from the drainage ditch and vegetating 10-metres from the creekbank of the drainage ditch with native plants known for nutrient uptake and cooling benefits of a shade canopy. Also, night lighting is to be directed away from the west forest patch off site (west of and adjacent to Yardistry lands).

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#### 4.1.2 Yardistry

The Yardistry development is proposed to occur on present day (interim) farmed land, implementing a 50-metre development setback from the westerly forest patch limits to provide a sufficient noise barrier for potential Eastern Wood Pewee using any of the forest patch to insure no disturbance. Additionally, enhancement plantings in the specimen hedgerow feature to bolster existing gaps is recommended. Night lighting is also to be directed away from the forest.

Overall, with the implementation of the recommended mitigation measures, no impacts to natural heritage features or Species at Risk habitat are anticipated.

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## 4.2 Archaeological Assessment

A Stage 1-2 Archaeological Assessment has been conducted for 255 Watson Drive (previously known as 275 Coral-Lea Drive) by AS&G Archaeological Consulting (May 3, 2025). The Assessment identified one historical Euro-Canadian site. The filed assessment and controlled surface pickup resulted in the collection of 138 artifacts from a surface scatter. Given that the land use history and artifact assemblage indicate a post-1870 occupation date for the site, it was not considered to have cultural heritage value or interest.

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## 4.3 Traffic Impact Brief

A Stage 1-2 Archaeological Assessment has been conducted for 255 Watson Drive (previously known as 275 Coral-Lea Drive) by AS&G Archaeological Consulting (May 3, 2025). The Assessment identified one historical Euro-Canadian site. The filed assessment and controlled surface pickup resulted in the collection of 138 artifacts from a surface scatter. Given that the land use history and artifact assemblage indicate a post-1870 occupation date for the site, it was not considered to have cultural heritage value or interest.

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### 4.3.1 Viking Cives

The TIB provides that the proposed development, with future expansion, is forecast to generate:

- 21 to 19 passenger vehicle trips during the AM and PM peak hour, respectively;
- Three and four truck trips during the AM and PM peak hour, respectively; and
- 27 and 27 passenger car equivalent net total trips during the AM and PM peak hour, respectively

Based on the forecast, Paradigm concluded that the proposed Viking Cives development is unlikely to impact traffic operations in a significant way and therefore did not recommend a full traffic impact assessment.

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### 4.3.2 Yardistry

The TIB provides that the proposed development is forecast to generate:

- 21 to 19 passenger vehicle trips during the AM and PM peak hour, respectively;
- One and two truck trips during the AM and PM peak hour, respectively; and

- 23 and 23 passenger car equivalent net total trips during the AM and PM peak hour, respectively

Based on the forecast, Paradigm concluded that the proposed Yardistry development is unlikely to impact traffic operations in a significant way and therefore did not recommend a full traffic impact assessment.

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## 4.4 Hydrogeological and Nitrates Study

GEI Consultants prepared a Scoped Hydrogeological Feasibility Study for both the Yardistry lands and the Viking-Cives lands. With regard to the Yardistry Lands, the Study concludes:

- The proposed lot is suitable for servicing with a standard Class IV or equivalent on-site sewage system with a low potential for impacting local water resources. Using the D-5-4 Guideline predictive assessment method, the resultant nitrate concentration for the entire proposed layout was calculated to be 6.90 mg/L, which is below the ODWS of 10 mg/L;
- Based on available well records in the surrounding area, sufficient groundwater is anticipated to be available from the bedrock aquifer system; and
- The hydrogeological conditions generally support the proposed development with a private sewage system and water supply with no impacts or interference with present water resources.

With regard to the Viking-Cives lands, the Study concludes:

- the proposed lot is suitable for servicing with a standard Class IV or equivalent on-site sewage system with a low potential for impacting local water resources. Using the D-5-4 Guideline predictive assessment method, the resultant nitrate concentration for the entire proposed layout was calculated to be 2.22 mg/L, which is below the ODWS of 10 mg/L
- based on available well records in the surrounding area, sufficient groundwater is anticipated to be available from the bedrock aquifer system,
- the hydrogeological conditions generally support the proposed development with a private sewage system and water supply well with no impacts or interference with present water resources.

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## 4.5 Servicing Options Report

Cobide Engineering Ltd. prepared a Servicing Options Report to evaluate servicing options for the proposed development of the Yardistry and Viking-Cives lands. The study concludes that:

1. Domestic water supply can be provided by on-site individual wells.
2. On-site sewage disposal and treatment systems are the preferred alternative as supported by the **Nitrate Attenuation (D-4-5) Study** completed by GEI Consultants.



3. Stormwater can be conveyed to the proposed stormwater management facilities by site grading and internal storm sewers. Further details of the proposed stormwater management concept will be outlined in the Stormwater Management Report completed by Cobide Engineering Inc.
4. The proposed internal roadways and parking lots within each individual development can be constructed to meet the Municipality of West Grey standards.
5. The proposed development can be serviced with natural gas, hydro, telephone, and internet.

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## 4.6 Preliminary Stormwater Management Report

Cobide Engineering Ltd. prepared a Preliminary Stormwater Management Report in support of these applications. The study concludes that:

- Stormwater quantity control will be provided via SWM Facilities with outlet configurations as detailed in the report.
- Stormwater quantity control for the development will maintain or lower pre-development flows for the 25-, 50- and 100-year storm events. The post development flows for the 2- and 5-year storm events at Discharge Point #2 will be higher than the pre-development events. This is not considered a concern at this discharge location, as the outlet discharges into an existing drainage ditch that would have been designed to accommodate and safely convey the existing flows from all storm events up to and including the 100-year storm event in order to protect the existing lots to the east of the drainage ditch.
- Stormwater quality will be provided by a treatment train approach which will include lot level control, conveyance control and 'end-of-pipe' control measures.
  - Lot level control will be provided by directing most impervious areas not directly connected to the municipal storm sewer system, over vegetated areas and directing as much overland drainage to grass swales prior to discharging into the proposed storm sewer system.
  - Conveyance control will be provided by and providing a minimum 600 mm sumps in all catchbasins and a minimum 300 mm sumps in all catchbasin manholes.
  - OGS systems will be utilized as End-of-pipe control as part of the individual site development plan for each lot.

All three levels of the treatment train approach will be used for the development to provide an "Enhanced" Level of protection for the development.

Note, legal/satisfactory outlet for both developments are to be obtained through future planning approvals. Letters in Principle were prepared for both the Viking Cives and Yardistry sites to outlet to adjacent properties.

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# 5.0 Planning Analysis

The proposed application must be assessed in terms of applicable policies prescribed by the Province and Grey County. The following evaluates this application in consideration of relevant policies of the Provincial Planning Statement and the Grey County Official Plan.

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## 5.1 Provincial Planning Statement (2024)

The Provincial Planning Statement, 2024 (PPS) was issued by the Province of Ontario in accordance with Section 3 of the Planning Act. The 2024 PPS applies to all decisions regarding the exercise of any authority that affects a land use planning matter made on or after October 20, 2024.

The PPS provides policy direction on matters of provincial interest related to land use planning and development. It provides a vision for land use planning in Ontario that encourages efficient use of land, resources and public investment in infrastructure. The PPS encourages a diverse mix of land uses to provide choice and diversity to create complete communities. A variety of modes of transportation are required to facilitate pedestrian movement, active transportation opportunities and less reliance on the automobile. The PPS strongly encourages development that will provide long term prosperity, environmental health and social wellbeing. The PPS identifies that planning decisions 'shall be consistent' with the Planning Statement.

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### 5.1.1 Settlement Area Expansions

As discussed earlier, a portion of the lands planned for development are located within the 'Industrial Business Park Settlement Area' on Schedule A – Map 3 to the County Official Plan. However, to accommodate the development of the Viking-Cives Expansion Lands and the Yardistry Lands, additional lands immediately adjacent to the Settlement Area boundary are required to be added to the Settlement Area.

Section 2.3 of the PPS provides policy direction for consideration of Settlement Area Boundary expansions. More specifically, Subsection 2.3.2 states that:

In allowing settlement area boundary expansions, planning authorities shall consider the following:

- The need to designate and plan for additional land to accommodate an appropriate range and mix of land uses;
- If there is sufficient capacity in existing or planned infrastructure and public service facilities;
- Whether the applicable lands comprise specialty crop areas;

- The evaluation of alternative locations which avoid prime agricultural areas and, where avoidance is not possible, consider reasonable alternatives on lower priority agricultural lands in prime agricultural areas;
- Whether the new or expanded settlement area complies with the minimum distance separation formulae;
- Whether the impacts on the agricultural system are avoided, or where avoidance is not possible, minimized and mitigated to the extent feasible as determined through an agricultural impact assessment or equivalent analysis, based on provincial guidance; and
- The new or expanded settlement area provides for the phased progression of urban development.

The following analysis considers the criteria:

**The need to designate and plan for additional land to accommodate an appropriate range and mix of land uses**

*Grey County & Municipality of West Grey*

Grey County undertook a Growth Strategy update in 2021 that provides updated population and employment projections through to the year 2046.

The County (based on the projections in the Growth Plan) projected a population of 127,130 by the year 2046, which represented a projected increase of 23,810 persons between 2021 and 2046. The Municipality of West Grey was projected to grow from 13,360 to 15,110, or an increase of 1,750. This represented approximately 7.4% of the County's projected new growth. Overall, West Grey accounted of approximately 12% of the overall County population in 2046.

With regard to employment, the Official Plan planned for an increase in 8,680 jobs between 2021 and 2046, of which 680 were allocated to West Grey (or 7.8%).

The province repealed the Growth Plan in October 2024, replacing it with the 2024 PPS, as noted above. The province also produced revised population projections through to the year 2051, which is approximately a 25-year planning horizon. Through these projections, Grey County was projected to have a population of 152,896 by the year 2051, which is an increase of 25,766 from the 2046 projection—the County will need to plan for this increase in population.

West Grey currently accounts for 12% of the County's population – which would result in the need to plan for 3,093 additional persons by 2051. If the lower allocation of 7.4% is used based on the last Growth Strategy update, West Grey will be required to plan for 1,906 persons. In this regard, West Grey will need to plan for between 1,906 and 3,093 additional persons by 2051.

There is a direct correlation between population and employment growth. In this regard, the previous projection that required additional land to accommodate employment growth within West Grey did not account for the additional population growth projected to 2051 and additional employment lands are required to accommodate this growth.

### *Wellington County & Township of Wellington North*

Although the lands subject to this application are located within Grey County and the Municipality of West Grey, they are located in close proximity to Wellington County and the Municipality of Wellington North.

Wellington County undertook a Municipal Comprehensive Review in 2022. That Review recommended that 40 hectares of land within Wellington North be declared as 'Excess Lands', concluding that the municipality already had more employment land than they would require. Phase 3 of the MCR is to examine these lands to determine their future planned land uses.

The Township of Wellington North prepared a Growth Management Plan in 2023. The Plan noted there was a surplus of employment lands in the municipality, but noted the following (emphasis added):

- While there is a surplus of Employment Area lands in the Township, it is important to recognize that a large portion of the vacant Employment Area land supply is not currently available for development or is not suitable for immediate use due to servicing and the current use of the sites;
- For example, a large portion of the Employment Area land supply in Mount Forest is used for agricultural purposes and the land is not currently marketed for industrial purposes. Over the long term, these lands may become available; however, in the short term, the Township has a limited supply of serviced Employment Area lands ready for industrial development.

In this regard, the adjacent municipality may have sufficient or excess lands designated for employment uses, but the lands are not currently available for employment uses and have constraints associated with their future potential development.

The majority of the lands designated 'Employment Area' in the County Official Plan within Mount Forest are located north of Highway 6 and are all currently farmed. While they may be 'available' for employment uses in the context of the Official Plan, they are currently farmed and as noted in the Township report, there are no current plans to redevelop them for employment uses.

The proposed Industrial Business Park Settlement Area expansion would positively contribute to development ready employment lands available in the short-term. Both Viking-Cives and Yardistry need employment land in the short term in order to remain in the area and contribute to the growth of the community. Viking-Cives requires land to accommodate their growing business and Yardistry requires land to relocate following the termination of their lease in order to keep this company in the community.

### **If there is sufficient capacity in existing or planned infrastructure and public service facilities**

The Servicing Options Study recommends that private individual services be used for the proposed development. The planned manufacturing buildings will be located in an area that is already serviced by private individual systems. The Servicing Options Study concludes that extending full municipal services through to service the manufacturing buildings is not recommended.

The Hydrogeological and Nitrates Study concludes that the manufacturing facilities can be appropriately serviced by private individual services.

## **Whether the applicable lands comprise specialty crop areas**

The lands proposed for inclusion within the Industrial Business Park Settlement Area do not comprise specialty crop areas.

The PPS 2024 defines specialty crop areas as:

areas designated using guidelines developed by the province, as amended from time to time. In these areas, specialty crops are the predominantly grown, such as tender fruits (peaches, cherries, and plums), grapes, other fruit crops, vegetable crops, greenhouse crops, and crops from agriculturally developed organic soil, usually resulting from:

- a) Soils that have suitability to produce specialty crops, or lands that are subject to special climatic conditions, or a combination of both;
- b) Farmers skilled in the production of specialty crops; and
- c) A long-term investment of capital in areas such as crops, drainage, infrastructure and related facilities and services to produce, store, or process specialty crops.

The lands and surrounding areas have not been identified or designated as a specialty crop area by the province or the municipality. The lands also do not exhibit characteristics of a specialty crop production as defined by the PPS. The soils have not been identified to have specific suitability for specialty crops, nor is there a history of specialty crops being grown on the lands. In summation, the subject lands do not exhibit any characteristics of a specialty crop area.

## **The evaluation of alternative locations which avoid prime agricultural areas and, where avoidance is not possible, consider reasonable alternatives on lower priority agricultural lands in prime agricultural areas**

The lands proposed for inclusion in the Settlement Area are designated Rural – they are not designated ‘prime agricultural area’.

## **Whether the new or expanded settlement area complies with the minimum distance separation formulae**

The Provincial Planning Statement 2024 requires that expanded settlement area boundaries comply with minimum distance separation (MDS) formula.

Within Rural and Prime Agricultural Areas, new non-farmland uses are required to meet the Minimum Distance Separation I (“MDS I”) formula as provided in “The Minimum Distance Separation Implementation Document: Formulae and Guidelines for Livestock Facility and Anaerobic Digester Odour Setbacks, Publication 853 of the Ontario Ministry of Agriculture, Food and Rural Affairs, 2016” (“MDS Guidelines”). The MDS I formulae applies to all existing livestock facilities and empty livestock facilities. An empty livestock facility means a facility that is no longer used to house livestock but appears to be reasonably capable of housing livestock. The MDS I formulae was not applied to facilities that are in poor or deteriorating conditions and determined to not be suitable for housing livestock.

As part of municipal consideration of planning or building permit applications, all existing livestock facilities or anaerobic digesters within a 1500 metre distance of a proposed Type B land use shall be investigated and MDS I setback calculations undertaken where warranted. In accordance with the MDS

Guidelines, settlement area expansions are considered a Type B land use (more sensitive) as settlement areas have a higher density of human occupancy, habitation, or activity which coincides with a higher potential for nuisance complaints.

The factors used to determine the MDS I setback requirements for these facilities include: the type of livestock; the maximum capacity of the barn for livestock; the type of manure system and the type of land use. These factors were determined through field observations undertaken during roadside visits, aerial imagery review, and property searches in business directories and local news sources.

OMAFA's AgMaps mapping application was used to calculate property size and barn size, where required, and the AgriSuite program was used to prepare the calculations.

See **Table 1** for a summary of MDS, and **Appendix B** for the MDS worksheets generated in AgriSuite and MDS setback maps generated in AgMaps.

**Table 1. MDS I Summary Table**

Address	Livestock Type	MDS	MDS met
415 Wellington County Road 6	Beef farm	272 m	Yes
6884 ON-89	Horse farm	191 m	Yes
42487 Grey County Rd 109	Dairy farm	430 m	Yes
		487 m	Yes
271087 County Road 6	Beef farm	204 m	Yes
*where only one MDS measurement is listed, that measurement applies to both the barn and manure storage (or barn only where there is no manure storage). Where two MDS measurements are listed, the first refers to the barn and the second to manure storage.			

The MDS I setback calculations completed for the livestock operations indicate that the proposed settlement area boundary expansion meets the setback requirements, and as such, complies with the minimum distance separation formulae.

The proposed industrial use is compatible with agricultural uses further to the southwest.

**Whether the impacts on the agricultural system are avoided, or where avoidance is not possible, minimized and mitigated to the extent feasible as determined through an agricultural impact assessment or equivalent analysis**

The lands are designated Rural Lands, which already permits non-agricultural uses. The subject lands are also well separated from surrounding agricultural uses by nature of the existing Industrial Business Park settlement area and industrial uses to the north and east, the Mount Forest settlement area to the south, and natural features to the west.

**Whether the impacts on the agricultural system are avoided, or where avoidance is not possible, minimized and mitigated to the extent feasible as determined through an agricultural impact assessment or equivalent analysis, based on provincial guidance**

There are no impacts on the agricultural system as the lands are not designated prime agricultural and are well separated from nearby agricultural uses.

**The new or expanded settlement area provides for the phased progression of urban development**

The expansion would effectively create a larger development parcel for the new Viking Cives manufacturing facility, a portion of which is located within the existing Settlement Area boundary. Also, it would create a development parcel for the Yardistry manufacturing facility adjacent to the Viking-Cives expansion lands to the east and the existing Industrial Business Park Settlement Area to the north. In this regard, it represents a logical and phased progression of the existing urban development in the immediate area.

Overall, it is our opinion that the proposed Industrial Business Park Settlement Area boundary expansion is consistent with the PPS 2024 criteria for settlement area boundary expansions.

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## 5.1.2 Employment

The employment section of the PPS (Section 2.8) provides that planning authorities shall promote economic development and competitiveness by:

- a) providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;
- c) identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment;
- d) encouraging intensification of employment uses and compatible, compact, mixed-use development to support the achievement of complete communities; and
- e) addressing land use compatibility adjacent to employment areas by providing an appropriate transition to sensitive land uses

The proposed applications seek to bring the subject lands into the Industrial Business Park Settlement Area and to rezone the lands to permit industrial uses. This will contribute to the achievement of the



above directives by contributing to the range of employment uses within the Municipality of West Grey and Mount Forest area and providing suitable market-ready sites that take into account the needs of existing businesses (Yardistry and Viking-Cives). Additionally, the subject lands are adjacent to existing industrial uses to the north, east, and south which provide an appropriate transition to, and separation from, existing sensitive uses within Mount Forest (the closest of which is more than 500 metres from the southern boundary of the subject lands) to the south and east. Appropriate transitioning to rural residential uses to the west (the closest of which is more than 700 metres from the western boundary of the lands) is achieved through separation by way of existing agricultural land and natural heritage features. This achieves PPS direction to ensure that land use compatibility is achieved.

Further, this section of the PPS directs that Planning authorities shall designate, protect and plan for all employment areas in settlement areas by:

- a) planning for employment area uses over the long-term that require those locations including manufacturing, research and development in connection with manufacturing, warehousing and goods movement, and associated retail and office uses and ancillary facilities;
- b) prohibiting residential uses, commercial uses, public service facilities and other institutional uses;
- c) prohibiting retail and office uses that are not associated with the primary employment use;
- d) prohibiting other sensitive land uses that are not ancillary to uses permitted in the employment area; and
- e) including an appropriate transition to adjacent non-employment areas to ensure land use compatibility and economic viability.

The proposed applications will bring the subject lands into an existing employment area (the Industrial Business Park Settlement Area). The lands are effectively surrounded by an existing industrial area, with industrial uses existing to the north, east, and south of the site. As discussed, the lands are well separated and appropriately transition from nearby non-employment uses. The proposed applications will satisfy a demand (by Viking-Cives and Yardistry) in the short-term for suitable market-ready industrial sites without compromising the long-term planned function of employment lands within West Grey.

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### 5.1.3 Land Use Compatibility

The land use compatibility section of the PPS (Section 3.5) seeks to appropriately plan for “major facilities” and “sensitive land uses” to avoid and/or minimize and mitigate potential adverse effects from odour, noise and other contaminants and minimize risk to public health and safety.

The PPS defines major facilities as:

facilities which may require separation from sensitive land uses, including but not limited to airports, manufacturing uses, transportation infrastructure and corridors, rail facilities, marine facilities, sewage treatment facilities, waste management systems, oil and gas pipelines, industries, energy generation facilities and transmission systems, and resource extraction activities.



The PPS defines sensitive land uses as:

means buildings, amenity areas, or outdoor spaces where routine or normal activities occurring at reasonably expected times would experience one or more adverse effects from contaminant discharges generated by a nearby major facility. Sensitive land uses may be a part of the natural or built environment. Examples may include, but are not limited to: residences, day care centres, and educational and health facilities.

The nearest sensitive land uses are greater than 500 metres from the subject lands (residential dwellings located at 310 Sligo Road West) and are separated from the subject lands by existing industrial uses [Waste Management Inc. (WM), automotive shops, and manufacturing uses]. Overall, adverse effects are not anticipated to result from the proposed dry manufacturing uses.

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#### 5.1.4 Sewage, Water and Stormwater

The PPS provides that “where municipal sewage services and municipal water services or private communal sewage services and private communal water services are not available, planned or feasible, individual onsite sewage services and individual on-site water services may be used provided that site conditions are suitable for the long-term provision of such services with no negative impacts” (Section 3.6).

Neither municipal servicing nor communal servicing is available or feasible on the subject lands. The Servicing Options Study recommends that private individual services be used for the proposed development. The planned manufacturing buildings will be located in an area that is already serviced by private individual systems. The Servicing Options Study concludes that extending full municipal services through to service the manufacturing building is not recommended.

The Hydrogeological and Nitrates Study concluded that the manufacturing facility can be appropriately serviced by private individual services.

As such, long-term provision of private servicing is not anticipated to have negative impacts.

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#### 5.1.5 Natural Heritage

Section 4.1 of the PPS contains policies that provide for the long-term protection of natural features and areas. Relevant to 255 Watson Drive, these policies prohibit development and site alteration in significant woodlands and wetlands, significant wildlife habitat, fish habitat, and habitat of endangered species and threatened species, and with adjacent lands to these features/areas unless the ecological function of the adjacent lands has been evaluated and it has been demonstrated that there will be no negative impacts on the natural features or on their ecological functions.

An EIS was prepared by SAAR Environmental Limited. The subject lands are located outside of natural features/areas and identified setbacks as provided in the EIS (see **Section 4.1** of this report for a summary of the EIS). With incorporation of the setbacks and mitigation measures provided in the EIS, the proposed applications are consistent with the natural heritage policies of the PPS.

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### 5.1.6 Cultural Heritage and Archaeology

With respect to cultural heritage and archaeology, the PPS identifies that significant built heritage resources and significant cultural heritage landscapes shall be conserved and development and site alteration shall not be permitted on lands containing archaeological resources or areas of archaeological potential unless significant archaeological resources have been conserved.

A Stage 1-2 Archaeological Assessment has been conducted for 255 Watson Drive by AS&G Archaeological Consulting and identified one historical Euro-Canadian site that was determined not to have cultural heritage value or interest (due to it being interpreted as a post-1870 occupation). A summary of its findings can be found in **Section 4.2** of this report.

**In conclusion, the Official Plan and Zoning By-law Amendment to expand the Settlement Area boundary and designate and zone lands for employment uses is consistent with the PPS.**

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## 5.2 Grey County Official Plan

The County of Grey Official Plan (County OP) is the upper tier planning document that guides planning policy and development on a County wide basis. The County OP provides for various goals and objectives relating to the County's roll in upper tier planning, including respecting the role and minimizing any adverse impact on the natural environment, providing for a desirable and efficient land use pattern, and ensuring sufficient lands are available for development to provide for employment and a mix of housing, among other matters.

The County OP includes a variety of goals, including minimizing adverse impacts on the natural environment and environmental features, protecting water quality, protecting archeological sites, and providing the most desirable, orderly, and efficient land use patterns. The goals of the County OP are to ensure that sufficient lands have been identified for development to accommodate a variety of mixed housing and employment opportunities to meet current and future needs.

The subject lands are currently designated 'Rural' by the County OP. A County Official Plan Amendment is required to redesignate the subject lands to Industrial Business Park Settlement Area designation to facilitate the proposed developments, as well as the proposed Industrial Business Park Settlement Area Expansion.

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### 5.2.1 Economic Objectives

The County Official Plan sets out economic objectives including encouraging a diverse economy while recognizing and promoting economic specialization in areas such as tourism, agriculture, manufacturing, retail, etc. and ensuring available lands, services, and zoning policies are in place to attract a wide range of investment and businesses (Section 3.2 of the Official Plan). Further, the Official Plan encourages that the designation of new lands for industrial purposes be done in a way to ensure compatibility with existing development and not detracting from existing areas of economic activity.

The proposed applications will help the County realize the above economic goals and objectives by supporting existing manufacturing businesses within the community and providing those businesses with suitable land to accommodate their growing needs. Additionally, the subject lands are adjacent to existing industrial uses and well setback from existing sensitive uses within Mount Forest and the surrounding rural area and are not designated prime agricultural.

With respect to the supply of employment lands, the Official Plan provides that the County will ensure that sufficient land is designated and available to accommodate an appropriate range and mix of employment opportunities and that the supply is adequate to meet future demand for up to 25 years (Section 3.2.2 of the Official Plan). It further provides that an adequate supply includes maintaining a range and various sizes of suitable sites for employment uses which support a wide range of economic activities and ancillary uses and take into account the needs of existing and future businesses.

As previously discussed, Grey County undertook a Growth Strategy update in 2021 that provided updated population and employment projections through to the year 2046. With regard to employment, the Official Plan planned for an increase in 8,680 jobs between 2021 and 2046, of which 680 were allocated to West Grey (or 7.8%).

The province produced revised population projections through to the year 2051, which is approximately a 25-year planning horizon. Through these projections, Grey County was projected to have a population of 152,896 by the year 2051, which is an increase of 25,766 compared with the 2046 projections – the County will need to plan for this increase in population. Based on the County's population allocation for West Grey will need to plan for between 1,906 and 3,093 additional persons by 2051. The previous projection that required additional land to accommodate employment growth within West Grey did not account for the additional population growth projected through to 2051 and additional employment lands are required to accommodate this growth.

As such, the proposed applications will create additional employment lands for immediate use by two existing local businesses that will contribute positively to jobs in the County and Municipality of West Grey.

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## 5.2.2 Settlement Area Boundary Expansion

Subsection 3.4.2 of the County Official Plan provides that “the County may identify a settlement area or allow the expansion of a settlement area designation only at the time of a comprehensive review or an updated comprehensive review”. In accordance with the 2024 PPS, comprehensive reviews are no longer required to consider a settlement boundary expansion request. The Official Plan also provides that the County may allow for the expansion of settlement area designation only where it is demonstrated that:

**It is based on a review of population and growth projections, and which reflect projections and allocations by the County; and considers alternative directions for growth; and determines how best to accommodate this growth while protecting provincial, County, and local interests;**

As previously discussed, based on the province's revised population projections through to the year 2051, there is a need for additional employment land in West Grey than what was accounted for in the

County' 2021 Growth Strategy. The proposed settlement area boundary expansion seeks to add 10.14 hectares into the Industrial Business Park Settlement Area, which is a relatively small amount of land, in response to the demands of two local manufacturing companies who require additional land to continue operating in the community. The subject lands are adjacent to the existing Industrial Business Park settlement area and represent the natural extension of this area.

**Sufficient opportunities for growth are not available through intensification, redevelopment and designated growth areas to accommodate projected needs over the identified planning horizon;**

There are no other investment ready parcels within the existing Industrial Business Park Settlement Area that are capable of accommodating the needs of Yardistry and Viking Cives (lot area, access, proximity to existing operations, ready for development in the short-term).

**The infrastructure and public service facilities which are planned or available are suitable for the development over the long term, are financially viable over their life cycle, which may be demonstrated through asset management planning; and protect public health and safety and the natural environment;**

**The area of the proposed development can be adequately serviced;**

**Sufficient water quality, quantity and assimilative capacity of receiving surface watercourse / water body and/or aquifer are available to accommodate the proposed development;**

**An analysis of servicing and transportation facilities, ensuring the efficient use and expansion of servicing infrastructure including potable water, sanitary sewage collection and treatment, sidewalks, trails, and transit;**

As previously discussed, municipal servicing and communal servicing are not available on the subject lands. The Servicing Options Study recommends that private individual services be used for the proposed development. The planned manufacturing buildings will be located in an area that is already serviced by private individual systems (within the Industrial Business Park Settlement Area). The Servicing Options Study concludes that extending full municipal services through to service the manufacturing building is not recommended.

The Hydrogeological and Nitrates Study concludes that the manufacturing facility can be appropriately serviced by private individual services.

As such, long-term provision of private servicing is not anticipated to have negative impacts.

**Expansion will be considered primarily in the Rural land use type; and where it can be demonstrated that no alternative exists, expansion into other land use types may be permitted. No expansion will be permitted into Special Agricultural land use type areas. In the case of Agricultural land use type areas, it must be demonstrated that there are no reasonable alternatives which would avoid prime agricultural areas, and no alternatives exist on lower priority agricultural lands;**

**Impacts on agricultural operations adjacent or close to settlement area expansions are mitigated to the extent feasible and expansion of the new or expanding settlement area is in compliance with the provincial minimum distance separation formulae;**

The lands are designated Rural Lands, which already permits non-agricultural uses. The subject lands are also well separated from surrounding agricultural uses by nature of the existing Industrial Business Park settlement area and industrial uses to the north and east, the Mount Forest settlement area to the south, and natural features to the west.

The MDS I setback calculations completed for the livestock operations indicate that the proposed settlement area boundary expansion meets the setback requirements, and as such, complies with the minimum distance separation formulae.

**Impacts on licensed aggregate operations, and Aggregate Resource Areas are mitigated to the extent feasible;**

The subject lands are not located within or adjacent to a mapped 'Aggregate Resource Area' nor are there any existing licences in proximity to the subject lands.

**Growth can be accommodated without unacceptable impacts on the natural environment as defined in Section 7 of this Plan, surrounding land uses, and within the constraints imposed by servicing;**

**Protecting natural features and ecological functions within the natural heritage system;**

An EIS was prepared by SAAR Environmental Limited. The subject lands are located outside of natural features/areas and identified setbacks as provided in the EIS (see **Section 4.1** of this report for a summary of the EIS). With incorporation of the setbacks and mitigation measures provided in the EIS, impacts to the natural environment are not anticipated.

**Cross jurisdictional issues are considered, where cross jurisdictional is interpreted to mean neighbouring municipalities both within Grey County, as well as neighbouring counties; and**

During the pre-consultation period, consultation with Grey County, Municipality of West Grey, Wellington County, and the Township of Wellington North was undertaken to discuss the proposal given the location of the property within the Municipality of West Grey (Grey County) and adjacent to the town of Mount Forest (Township of North Wellington – Wellington County). Wellington County and Wellington North did not have concerns with the proposed Official Plan and Zoning By-law Amendment.

**Avoiding hazardous lands and hazardous sites;**

The subject lands (where development is proposed) are not identified as hazardous lands or as a hazardous site.

**Evaluating potential cultural heritage resources and conservation of significant built heritage resources, significant heritage landscapes and significant archaeological resources, all in keeping with the policies of this Plan.**

A Stage 1-2 Archaeological Assessment has been conducted for 255 Watson Drive by AS&G Archaeological Consulting and identified one historical Euro-Canadian site that was determined not to

have cultural heritage value or interest (due to it being interpreted as a post-1870 occupation). A summary of its findings can be found in **Section 4.2** of this report.

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### 5.2.3 Industrial Business Park Settlement Area

The subject lands are proposed to be brought into the Industrial Business Park Settlement Area. The Industrial Business Park Settlement Area land use type applies to those lands just north of Mount Forest off of Highway 6 in the Municipality of West Grey. The primary use of these lands must be for dry industrial uses. Permitted uses include (Section 3.10.1 of the Official Plan):

- Fuel distribution;
- Agricultural bulk sales establishment;
- Warehousing;
- Transport terminal;
- Dry manufacturing;
- Business or professional centre;
- Heavy equipment sales and rental;
- Industrial mall;
- Bulk sales establishment;
- Brewing or retail outlet; and
- Building supply outlet.

With the exception of 3.9.4(1) and 3.9.2(2), the development criteria of Section 3.9.4 apply to the Industrial Business Park Settlement Area land use type. With respect to these criteria, the proposed developments propose safe access onto existing local roads, will incorporate landscaping and site lighting to provide for aesthetically acceptable development, are dry in nature, will be serviced by individual well and septic systems (see Section 4.3 for summary of the Servicing Options Report), will incorporate adequate stormwater management (see Section 4.3 for a summary of the Stormwater Servicing Report), meets the minimum lot size requirements per the Zoning By-law, and will be subject to Site Plan Control.

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### 5.2.4 Natural Grey

The subject lands are not mapped as part of the County's Natural Heritage System (per Schedule C). Appendix B of the Official Plan maps unevaluated wetlands to the northwest and west of the subject lands. Per Section 7.3.2 of the Official Plan, no development or site alterations are permitted within other wetlands or their adjacent lands (30 m) or as identified by a conservation authority, unless it has

been demonstrated that there will be no negative impacts on the natural features or their ecological functions.

An EIS was prepared by SAAR Environmental Limited. The portion of the subject lands proposed for inclusion in the settlement area are located outside of natural features/areas and identified setbacks as provided in the EIS (see **Section 4.1** of this report for a summary of the EIS). With incorporation of the setbacks and mitigation measures provided in the EIS, the proposed applications are consistent with the natural heritage policies of the PPS and County Official Plan.

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### 5.2.5 Move Grey

The Yardistry lands are proposed to be accessed via Viola May Crescent (Municipal Local Road) and the Viking Cives lands via Harry Bye Boulevard (Municipal Local Road).

**In conclusion, the proposed Official Plan and Zoning By-law Amendment meets the intent of the Grey County Official Plan, with due consideration to updated policies in the 2024 PPS regarding expansions to settlement areas.**

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## 5.3 West Grey Zoning By-law

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### 5.3.1 Existing Zoning

255 Watson Drive is currently zoned 'Restricted Rural' (A3) and Natural Environment (NE). Both the Yardistry Lands and the Viking-Cives Expansion Lands are proposed within the portion of the site zoned A3. The A3 zone does not permit industrial uses. As such, a Zoning By-law amendment is required to permit industrial uses on the site.

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### 5.3.2 Proposed Zoning

The portion of the Viking Cives site that is already within the settlement area is zoned 'Industrial Zone' with site specific exception 237 (M1-237).

The proposed Zoning By-law amendment seeks to rezone the balance of the lands to the M1-237 zone to permit the proposed industrial use in alignment with the zoning of Assessment Parcel 420501000702430 and the zoning of the industrial settlement area to the north and northwest.

The M1-237 zone permits the following uses (bold emphasis added to indicate uses relevant to the proposal):

- Fuel distributor
- Agricultural bulk sales establishment
- Transport terminal

- Building supply outlet
- Warehousing
- Equipment sales and rental
- Automobile sales establishment
- Dry manufacturing plant
- Sawmill
- Public garage or body shop (agricultural machinery or vehicles)
- Automobile services station
- Farm equipment sales establishment
- A horticultural nursery
- Outdoor recreational equipment sales and service establishment
- Motel
- Open storage accessory to a permitted use
- Uses, buildings and structures accessory to a permitted use, including a business office and retail store.

The regulations of Section 24 (M1 zone) apply except for however for the following:

Minimum lot area: 0.8 hectares

Minimum front yard: 15 metres

Maximum building height: 15 metres

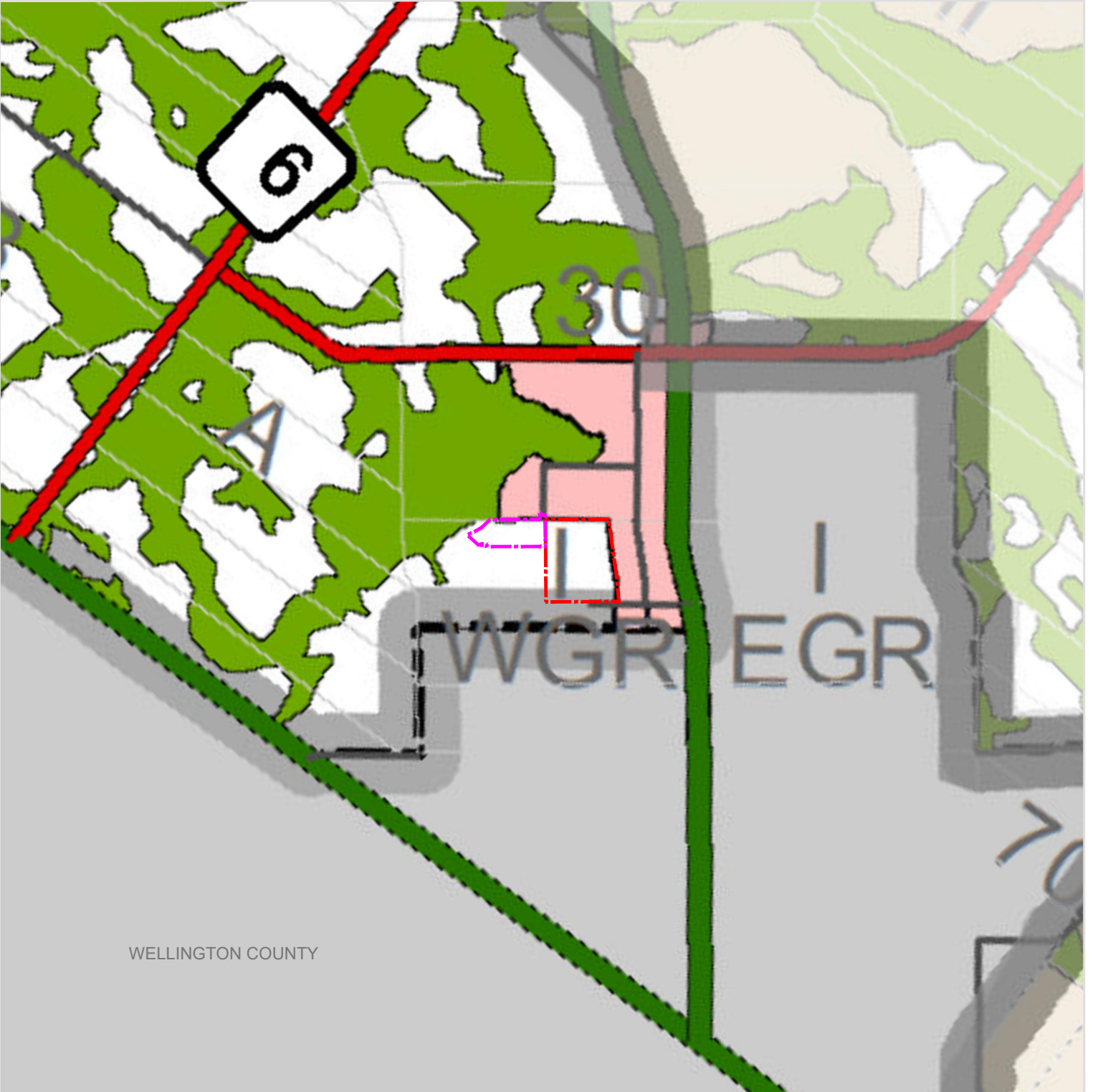
Minimum landscaped open space: 10%

Proposed Zoning: M1-237			
Provisions (Section 24.2)	Regulation	Proposed	
		Yardistry	Viking-Cives
Minimum lot area	800 sq. m (0.8 ha)	2.53 ha	9.06 ha
Minimum lot frontage	20 metres	20 metres (Viola May Road allowance width)	110.34 m
Minimum front yard	15 metres	46.27 m	17.07 m
Minimum interior side yard	3 metres	61.12 m (east); 30.75 m (west)	26.07 m (east); 41.51 m (west)



<b>Proposed Zoning: M1-237</b>			
<b>Provisions (Section 24.2)</b>	<b>Regulation</b>	<b>Proposed</b>	
		<b>Yardistry</b>	<b>Viking-Cives</b>
Minimum exterior side yard	7.5 m	N/A	N/A
Minimum rear yard	7.5 m	23.18 m	365.79 m
Maximum lot coverage	60%	18.36%	4.04 %
Maximum building height	15 metres	9.2 metres	9.5 m
Minimum landscaped open space	10%	47.13%	10.01%
<b>Provisions (Section 6.16)</b>	<b>Regulation</b>	<b>Yardistry</b>	<b>Viking-Cives</b>
Loading	2 spaces	2 spaces	2 spaces
<b>Provisions (Section 6.27)</b>	<b>Regulation</b>	<b>Yardistry</b>	<b>Viking-Cives</b>
Barrier Free Parking	1 for 20 spaces + 1/50 additional spaces	3 required, 3 spaces provided	2 required; 4 spaces provided
Parking	1/90 sq. m GFA + 3 visitor	51 required, 73 provided	48 required, 70 spaces provided
<b>Provisions (Section 6.26)</b>	<b>Regulation</b>	<b>Yardistry</b>	<b>Viking-Cives</b>
Outdoor storage	a) Such storage area shall not be located in any required yard setback.	N/A	complies
	b) Such storage area shall not comprise more than 40% of the total lot area.	N/A	To be determined at site plan
	c) Such storage area shall be visually screened from view from the street and from any abutting land zoned or used for residential, institutional or open space by a buffer area or strip in accordance with the applicable regulations of Section 6.3 of this By-law.	N/A	To be confirmed through site plan application



The Zoning By-law Amendment proposes to maintain the same zone that applies to multiple properties within the same settlement area. In our opinion, the M1-237 is appropriate for the planned development of the Viking Cives expansion and the Yardistry manufacturing use.



**Figure 6:**  
**Grey County**  
**Official Plan-**  
**Schedule A:**  
**Land Use Types**  
**Map 3**

PT LT 32 CON 1 DIVISION 1 NORMANBY;  
PT LT 32 CON 1 DIVISION 2 NORMANBY  
PT 7, 8 & 9 17R2039 AND PT 1 17R2090;  
T/W GS171216; WEST GREY

**LEGEND**

-  Viking-Cives Expansion Lands
-  Yardistry Lands

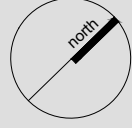
- |  |  |
|--|--|
|  Provincial Highway Connecting Link |  Agricultural                             |
|  Provincial Highway                 |  Industrial Business Park Settlement Area |
|  County Road                        |  Hazard Lands                             |
|  Local Road                         |  |
|  Seasonal Road                      |  |

**DATE:** February, 2025

**SCALE:** 1:20,000

**FILE:** 24367A

**DRAWN:** GC

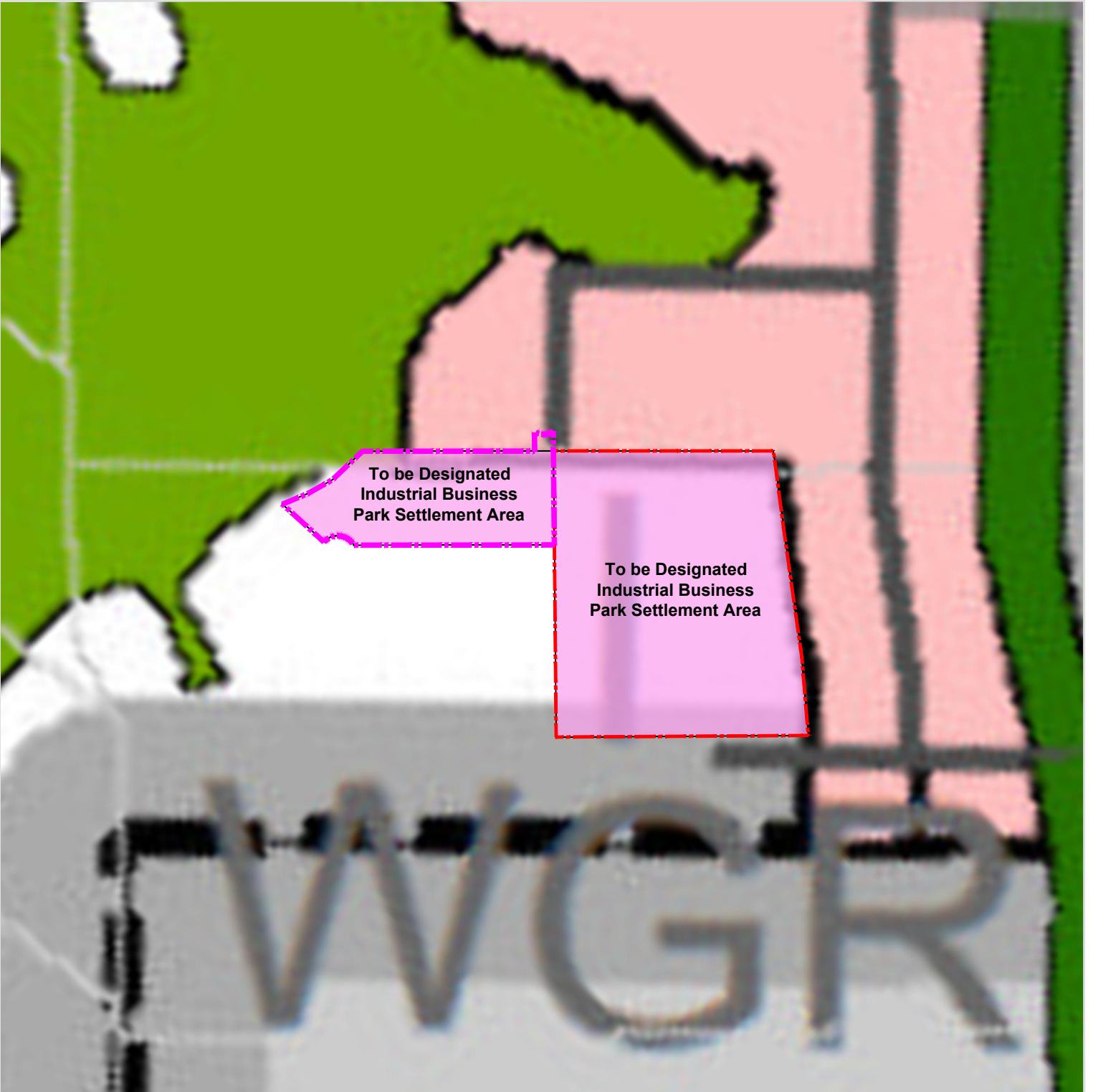


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**PLANNING  
URBAN DESIGN  
& LANDSCAPE  
ARCHITECTURE**

200-540 BINGEMANS CENTRE DR, KITCHENER, ON, N2B 3J9 | P: 519.576.3650 | WWW.MHBCPLAN.COM



**Figure 7:  
Official Plan  
Amendment**

PT LT 32 CON 1 DIVISION 1 NORMANBY;  
PT LT 32 CON 1 DIVISION 2 NORMANBY  
PT 7, 8 & 9 17R2039 AND PT 1 17R2090;  
T/W GS171216; WEST GREY

**LEGEND**

  Viking-Cives Expansion Lands


  Yardistry Lands

  Lands to be Designated Industrial Business Park Settlement Area

<span style="color: green;">—</span> Provincial Highway	<span style="background-color: #d2b48c; border: 1px solid black; padding: 2px;"> </span> Agricultural
<span style="color: green;">—</span> Connecting Link	<span style="background-color: #f08080; border: 1px solid black; padding: 2px;"> </span> Industrial Business Park Settlement Area
<span style="color: green;">—</span> Provincial Highway	<span style="background-color: #90ee90; border: 1px solid black; padding: 2px;"> </span> Hazard Lands
<span style="color: red;">—</span> County Road	
<span style="color: black;">—</span> Local Road	
<span style="color: black;">- - - - -</span> Seasonal Road	

<b>DATE:</b>	February, 2025
<b>SCALE:</b>	NTS
<b>FILE:</b>	24367A
<b>DRAWN:</b>	GC

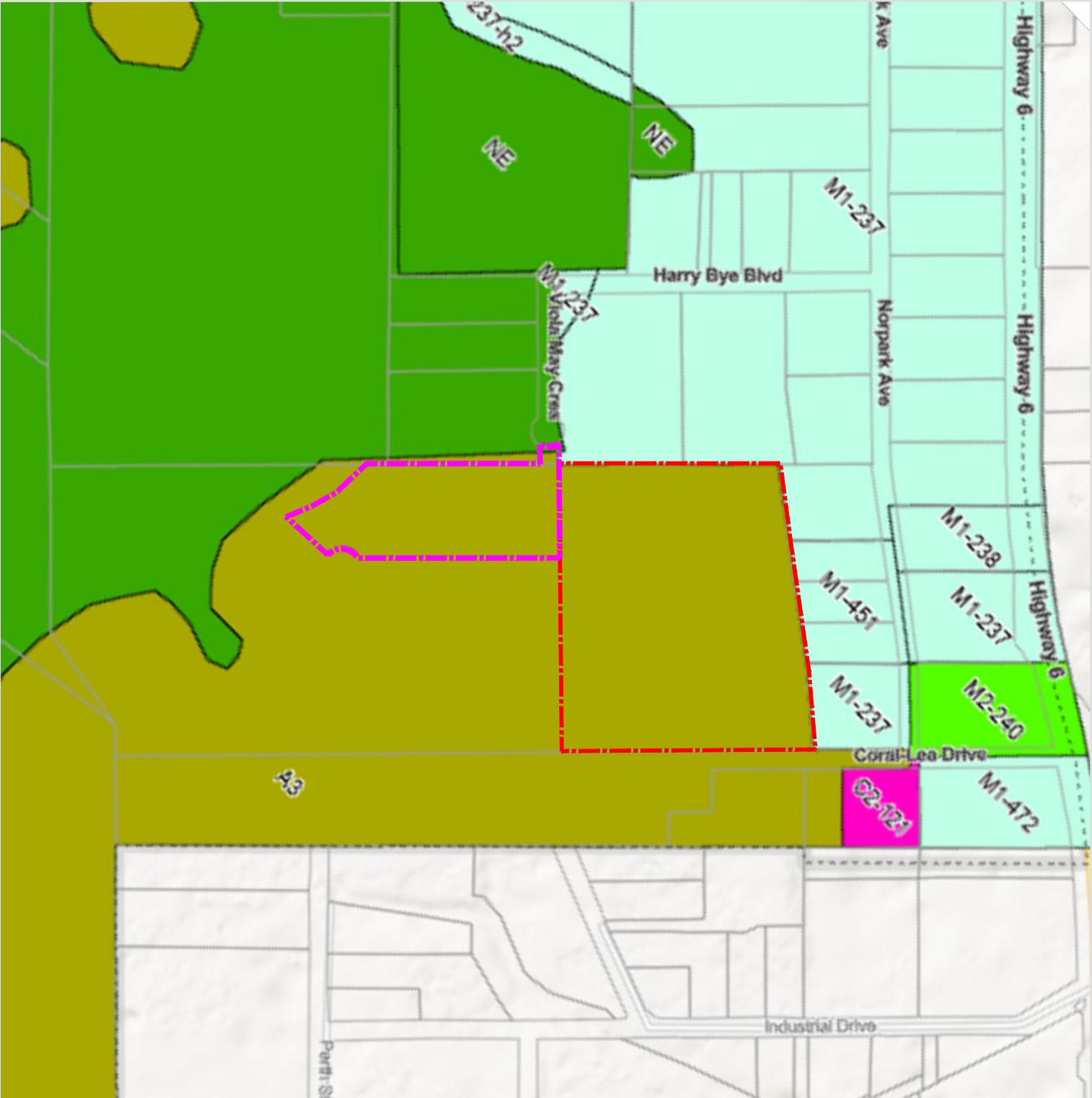
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PLANNING  
URBAN DESIGN  
& LANDSCAPE  
ARCHITECTURE

200-540 BINGEMANS CENTRE DR, KITCHENER, ON, N2B 3J9 | P: 519.576.3650 | WWW.MHBCPLAN.COM





**Figure 8:  
Municipality of  
West Grey  
Existing Zoning**

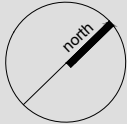
**LEGEND**

Viking-Cives Expansion Lands

Yardistry Lands

<span style="display: inline-block; width: 15px; height: 15px; background-color: #90EE90; border: 1px solid black; margin-right: 5px;"></span> A3 Restricted Rural
<span style="display: inline-block; width: 15px; height: 15px; background-color: #ADD8E6; border: 1px solid black; margin-right: 5px;"></span> M1 Industrial
<span style="display: inline-block; width: 15px; height: 15px; background-color: #00CED1; border: 1px solid black; margin-right: 5px;"></span> M2 Restricted Industrial
<span style="display: inline-block; width: 15px; height: 15px; background-color: #FF69B4; border: 1px solid black; margin-right: 5px;"></span> C2 Highway Commercial
<span style="display: inline-block; width: 15px; height: 15px; background-color: #3CB371; border: 1px solid black; margin-right: 5px;"></span> NE Natural Environment

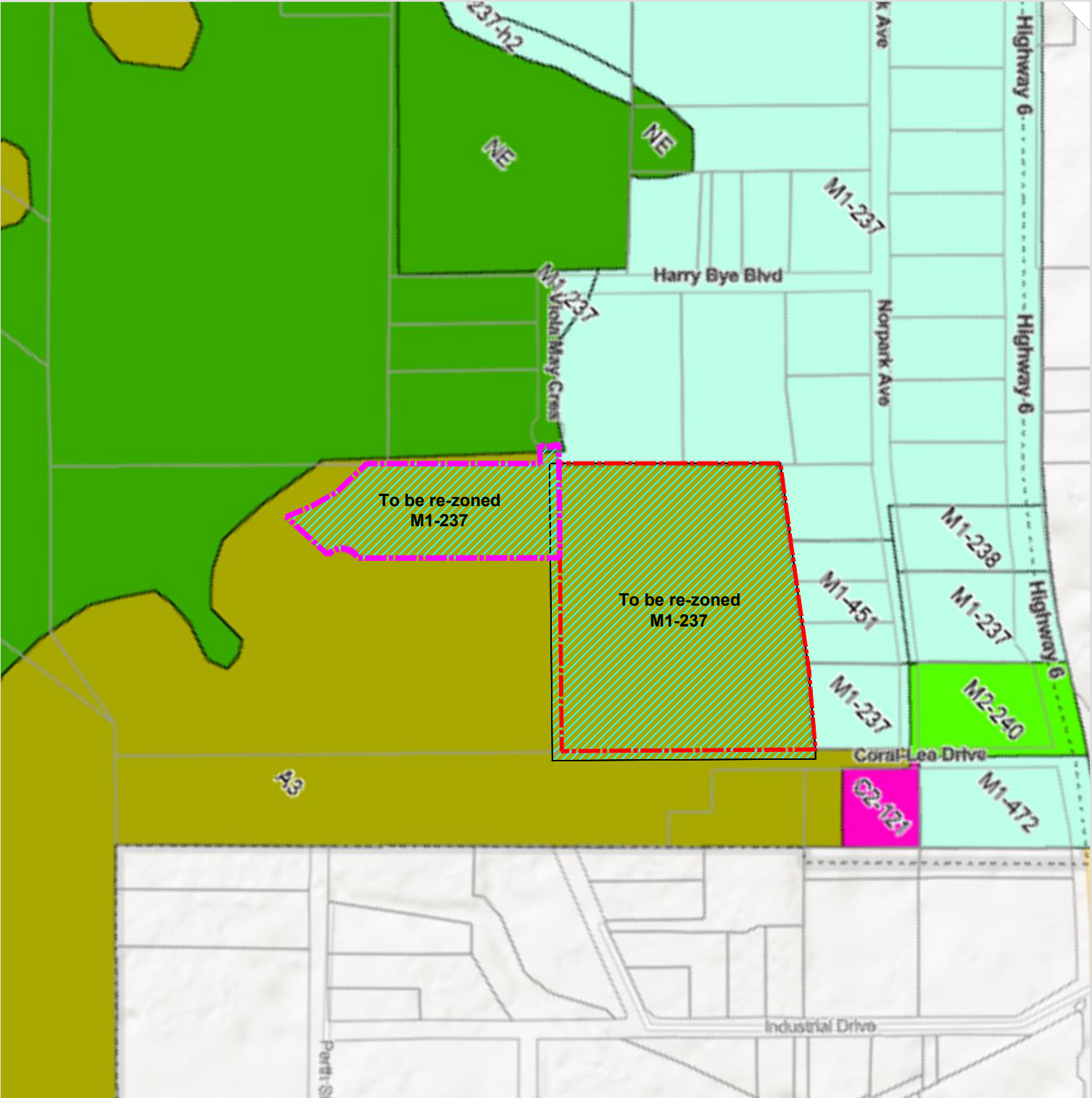
DATE:	February, 2025
SCALE:	NTS
FILE:	24367A
DRAWN:	GC



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PT LT 32 CON 1 DIVISION 1 NORMANBY;  
PT LT 32 CON 1 DIVISION 2 NORMANBY  
PT 7, 8 & 9 17R2039 AND PT 1 17R2090;  
T/W GS171216; WEST GREY

Source: West Grey Interactive GIS website.



**Figure 9:  
Zoning By-law  
Amendment**

**LEGEND**

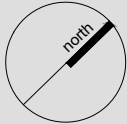
-  Viking-Cives Expansion Lands to be re-zoned from A3 to M1-237
-  Yardistry Lands to be re-zoned from A3 to M1-237
-  A3 Restricted Rural
-  M1 Industrial
-  M2 Restricted Industrial
-  C2 Highway Commercial
-  NE Natural Environment

**DATE:** February, 2025

**SCALE:** NTS

**FILE:** 24367A

**DRAWN:** GC



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# 6.0 Summary & Conclusions

As outlined in this Report, accompanied by the supporting technical studies, plans and reports, the proposed Official Plan and Zoning By-law amendment is appropriate and represents good planning for the following reasons:

- The proposed Official Plan Amendment and Zoning By-law Amendment is consistent with the Provincial Planning Statement (2024);
- The proposed applications meet the intent of the Grey County Official Plan, with consideration to updated population projections and settlement area boundary expansion policies included in the 2024 PPS;
- The proposed applications support the County in achieving employment targets and will provide land immediately needed by local businesses;
- The subject lands are well situated for the proposed industrial uses considering their proximity to existing industrial uses (the Industrial Business Park Settlement Area to the north, east, and south) and will contribute employment options within the County and West Grey;
- No impacts to adjacent natural heritage features are anticipated with the implementation of the recommendations provided in the EIS as summarized in Section 4.1;
- The Servicing Options Study recommends that private individual services be used for the proposed development. The planned manufacturing buildings will be located in an area that is already serviced by private individual systems.
- The Hydrogeological and Nitrates Study concludes that the industrial uses can be appropriately serviced by private individual services.

Accordingly, the proposed Official Plan and Zoning By-law Amendment will permit industrial development that contributes to employment options in the community, are compatible with the surrounding area, do not threaten agricultural or aggregate resource development, and make appropriate use of private servicing.

Respectfully submitted,

**MHBC**



Trevor Hawkins, M.PL, MCIP, RPP  
Partner



Chelsea Brooks, MA, MSc(PI.), RPP, MCIP  
Intermediate Planner

# A

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## Appendix A: Pre-consultation Record



# Planning and Development

595 9<sup>th</sup> Avenue East, Owen Sound Ontario N4K 3E3  
519-372-0219 / 1-800-567-GREY / Fax: 519-376-7970

December 04, 2024

Mr. Harry Bye

H. Bye Construction Ltd.

Sent via email to [h.bye@hbyeconstruction.com](mailto:h.bye@hbyeconstruction.com)

**RE: Pre-submission Consultation on Viking Cives Expansion and Randharr Business Park Expansion  
Municipal Roll Numbers 420501000702430 and 420501000702500  
Municipality of West Grey**

Dear Mr. Bye,

This correspondence is in response to our meeting on November 27, 2024. Thanks to your team for taking the time to meet with us to outline your proposed development plans, and for considering development in West Grey and Grey County. We offer the following comments as a written record of our discussions from the 27<sup>th</sup>. After you have had a chance to review this letter, staff are open to discussing this matter further should you have any questions. We look forward to working with you throughout the development process related to your proposals.

At the meeting we generally discussed the following two items:

1. the expansion plans of Viking Cives, and
2. the expansion of the Randharr Business Park.

## Background

The subject lands are currently designated and zoned as follows:

- 420501000702430 – designated Industrial Business Park Settlement Area and zoned as Industrial with Exceptions (M1-237), and
- 420501000702500 – designated Rural and Hazard Lands and zoned as Restricted Rural (A3) and Natural Environment (NE).

Appendix B to the County Plan also maps watercourses and other identified wetlands on or adjacent to the subject properties. Portions of the subject lands also fall within the Saugeen Valley Conservation Authority's (SVCA) Approximate Regulated and Screening Areas.



This letter will also make reference to lands owned by the Township of Wellington North, in the Municipality of West Grey, which are identified by municipal roll number 420501000702590.

As per our discussion at the meeting, staff understand that there is more urgency on the Viking Cives expansion. As a result, staff have broken the letter up into two components, (1) the requirements for the Viking Cives expansion, and (2) the requirements for the Randharr Business Park expansion.

### **Viking Cives Expansion**

The concept plan shared at the meeting showed two parcels of lands for the Viking Cives expansion as follows:

- a) 420501000702430 – approximately 5 acres, and
- b) a portion of 420501000702500 – approximately 19 acres.

Staff would note that parcel (a) above is already zoned and designated for such development, with frontage on an open and maintained municipal road. As such, should Viking Cives wish to develop on these lands, only a site plan control application and future building permits would be needed from the Municipality of West Grey. Note that permits or approvals may also be needed from SVCA or the Ministry of Transportation (MTO) on these lands.

For the portion of parcel (b) noted above, it would require an official plan amendment (OPA) to the County Official Plan, a zoning by-law amendment (ZBLA) to the West Grey Zoning By-law, as well as future site plan control and consent applications. A consent application would be required to sever the ~19 acres from the current lands (420501000702500) for the purposes of adding these lands onto parcel 420501000702430. West Grey also requires that the proponent provide a letter from their legal counsel regarding whether a deeming by-law can be passed for the purposes of merging the parcels.

With respect to the development of parcel (b) staff would note that this parcel does not currently have access to an open and maintained municipal road, as Coral-Lea Drive has not yet been extended to the southern boundary of this property. Staff would further note that the future Coral-Lea Drive extension appears to be on parcel 420501000702590 which is owned by the Township of Wellington North. As a result, unless the portion of 420501000702500 is added to 420501000702430, and access is provided by Harry Bye Boulevard, then the ~19 acres do not have access to an open and maintained municipal road.

Should you wish to apply for an OPA and ZBLA, the following studies and reports would be required:

- 1) Planning Justification Report(PJR)<sup>(i)</sup>,
- 2) Conceptual Site Plan showing the access(es) to the site, as well as the approximate proposed development on-site,
- 3) Servicing Options Study,
- 4) Functional Servicing Report (FSR) or Hydrogeologic and Nitrate Studies<sup>(ii)</sup>, if the servicing options study recommends the use of individual private services,
- 5) Stormwater Management (SWM) Report,
- 6) Archaeological Assessment,
- 7) Environmental Impact Study,
- 8) Traffic Impact Study (TIS) or Opinion Letter<sup>(iii)</sup>, depending on the requirements of the Ministry of Transportation (MTO), and
- 9) Documented pre-submission consultation with MTO, Wellington North, and Wellington County.

Some of the above studies and technical reports would be to demonstrate 'proof of concept' level feasibility for the OPA and ZBLA. Additional details may then be needed at the site plan control stage (e.g., stormwater management), should the OPA and ZBLA be approved.

(i)The PJR will need to address the settlement area expansion policies of both the Provincial Planning Statement (PPS) 2024 and the County of Grey Official Plan. More details on the level of justification required for settlement area expansions will be required later in this letter.

(ii)Should the Servicing Options Study conclude that the site can be serviced via full municipal services, then a FSR would be required, and Hydrogeologic and Nitrate Studies would not be required. The servicing options study should consider all servicing options as per the PPS and Ministry of the Environment, Conservation and Parks (MECP) guidelines. Staff are aware that currently servicing does not cross the municipal/county boundary, however the feasibility of such an extension should be considered as part of this study. Should the proposed expansion be serviced via full municipal services, then exceptions to the County's Industrial Business Park Settlement Area could be considered which allow for a wider array of industrial uses, beyond the dry industrial uses currently permitted under this designation.

(iii)At a minimum staff would be looking for a summary of the approximate traffic levels entering/exiting the site, broken out by trucks vs. automobiles. Depending on the traffic levels associated with the site, or based on pre-submission consultation with the MTO, then a full TIS may or may not be warranted. MTO is encouraging proponents to use

their portal for pre-submission consultation which can be found here:

<https://www.hcms.mto.gov.on.ca/PreConsultation>

## Randharr Business Park Expansion

In drafting this letter, staff have assumed that any OPA application could precede any ZBLA and draft plan of subdivision applications. Table 1 below outlines the information requirements should the applications proceed in this sequence. Should you proceed with the OPA, ZBLA, and plan of subdivision applications all at once, then all of the information requirements would be needed at the outset as part of complete applications.

**Table 1: Studies required in support of future development applications for the Randharr Business Park Expansion**

Item #	Study Type	County OPA	Zoning By-law Amendment / Plan of Subdivision
1	Planning Justification Report	Yes	Yes
2	Concept Plan	Yes	No
3	Servicing Options Study	Yes	No
4	Functional Servicing Report or Hydrogeologic and Nitrate Studies <sup>(A)</sup>	Yes	Yes
5	Traffic Impact Study <sup>(A)</sup>	Yes	Yes
6	Archaeological Assessment <sup>(B)</sup>	Maybe	Yes
7	Stormwater Management (SWM) Report	No	Yes
8	Environmental Impact Study (EIS) <sup>(C)</sup>	Yes	Yes
9	Draft Plan of Subdivision	No	Yes
10	Documented pre-submission consultation with MTO, Wellington North, and Wellington County <sup>(A)</sup>	Yes	Yes

- A. Demonstrating principle of development (or proof of concept) at the OPA stage, and further refined at the ZBLA and subdivision stage.
- B. Subject to early engagement with First Nations and Metis, this requirement may be deferred to the ZBLA and subdivision stage, unless First Nations or Metis recommend otherwise.
- C. Should sufficient detail be provided in the EIS at the OPA stage, and should the ZBLA and subdivision follow the OPA in short succession, then further assessments may not be required at the zoning and subdivision stage.

Where similar materials are noted in both the OPA and ZBLA or draft plan of subdivision stages, it may be possible to simply re-submit or reuse existing studies will be sufficient to satisfy Municipal/County requirements. If circumstances require, minor updates or addendums can be provided.

Similar to the above Viking Cives expansion, should the proposed expansion be serviced via full municipal services, then exceptions to the County's Industrial Business Park Settlement Area could be considered which allow for a wider array of industrial uses, beyond the dry industrial uses currently permitted under this designation.

### **Engagement with Wellington North and Wellington County**

The concept plan shared at the meeting showed roads being extended from the Township of Wellington North, as well as roads being built on lands in West Grey, owned by Wellington North. For this reason, as well as for reasons related to servicing expansions and land supply / demand purposes, early consultation with Wellington North and Wellington County will be required ahead of the submission of complete applications to Grey County and West Grey.

Particularly as it relates to the full Randharr Business Park expansion, there are significant impacts on the abutting Wellington North owned lands (420501000702590). Should this full expansion OPA proceed, there may be merit in a joint OPA with the Township of Wellington North to consider also designating their lands as part of the Industrial Business Park Settlement Area. If there is sufficient justification for a settlement area expansion, staff would prefer that parcels 420501000702500 and 420501000702590 proceed in tandem, such that the impacts can be considered in a comprehensive fashion versus on an individual basis.

### **Comments on Official Plan Amendment Applications for Settlement Boundary Expansions**

The 2024 Provincial Planning Statement (PPS) is now in force and effect. Given the changes to PPS 2024, a formal Comprehensive Review is no longer required to propose an expansion to a settlement area. That said, section 2.3.2 of the PPS will

need to be addressed as part of the OPA applications. Staff would note that the capacity of existing / planned infrastructure, including water supply and wastewater treatment will be key elements to address as part of this assessment. In assessing the requirements of section 2.3.2 of the PPS, the studies shall consider both the Municipality of West Grey and the Township of Wellington North (i.e., land supply / demand, as well as infrastructure in both municipalities).

Furthermore, PPS 2024 has also changed how municipalities project future growth (section 2.1.1). The County is planning on updating its growth management strategy in 2025 to address the Ontario Population Projections published by the Ministry of Finance. As part of this exercise, staff anticipate that the County's growth allocations to its nine member municipalities will also change within the County Official Plan. This could impact the numbers proposed in the PJR. Any PJR submitted should consider the most recent growth allocations available.

With respect to the Viking Cives proposed expansion, staff see the assessment as being more scoped in nature than the full Randharr Business Park expansion. Staff recommend that a draft terms of reference for the PJR be shared with West Grey, Grey County, Wellington North, and Wellington County ahead of completing such analyses.

### **General Notes and Reminders**

As part of the TIS, staff recommend early consultation with the Ministry of Transportation (MTO), in addition to Municipal and County Transportation staff. A terms of reference for this study should be shared with all applicable road authorities ahead of commencing the TIS.

Consultation with the Saugeen Valley Conservation Authority (SVCA) is recommended. County Planning Ecology staff will also be engaged in further consultation on these matters.

Tree removal in advance of any approvals of development shall only be done in accordance with the County's Forest Management By-law, and any applicable by-laws or regulations at the Municipality or Conservation Authority. A link to the County's Forest Management By-law can be found below:

<https://www.grey.ca/by-laws>

The County has created a series of technical guides, subdivision guidelines, and a healthy development checklist. County staff recommend reviewing these documents ahead of preparing any development plans. The documents are available at the below link:

<https://www.grey.ca/government/land-use-planning/planning-application-form-guidelines>

If you are considering submitting any development applications for these lands, County and Municipal staff recommend early engagement with First Nations and Metis peoples. If you require any contact information for Metis or First Nations just let us know and we can provide it.

The County and Municipality reserve the right to request further information based on public or agency comments or detailed technical review after the application(s) are submitted (if applications are submitted).

If you wish to discuss this matter further, please contact County or Municipal staff.

Yours truly,



David Smith, MCIP, RPP  
Manager of Planning  
Municipality of West Grey



Scott Taylor, MCIP, RPP  
Director of Planning and Development  
County of Grey

Cc Randy Bye, H. Bye Construction Ltd. (via email)  
Trevor Hawkins, MHBC Planning (via email)  
Chelsea Brooks, MHBC Planning (via email)  
David Smith, Municipality of West Grey (via email)  
Michele Harris, Municipality of West Grey (via email)  
Becky Hillyer, County of Grey (via email)


# B

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## Appendix B: MDS Calculations & Setback Map

MDS I - H. Bye

General information

Application date Mar 18, 2025	Municipal file number	Proposed application New or expanding settlement area boundary
Applicant contact information  ON	Location of subject lands County of Grey Township of West Grey NORMANBY Concession 1 WEST OF OWEN SOUND ROAD , Lot 32 Roll number: 4205010007025000000	



Calculations

415 Wellington County Rd 6

Farm contact information

ON

Location of existing livestock facility or anaerobic digester

County of Grey  
Township of West Grey  
NORMANBY  
Concession A WEST OF OWEN SOUND ROAD ,  
Lot 73  
Roll number: 4205

Total lot size

53 ha

Livestock/manure summary

Manure Form	Type of livestock/manure	Existing maximum number	Existing maximum number (NU)	Estimated livestock barn area
Solid	Beef, Backgrounders (7 - 12.5 months), Confinement	90	30 NU	418 m²

Confirm Livestock/Manure Information (415 Wellington County Rd 6)

The livestock/manure information has not been confirmed with the property owner and/or farm operator.

Setback summary

Existing manure storage	No storage required (manure is stored for less than 14 days)		
Design capacity	30 NU		
Potential design capacity	30 NU		
Factor A (odour potential)	0.8	Factor B (design capacity)	220
Factor D (manure type)	0.7	Factor E (encroaching land use)	2.2

Building base distance 'F' (A x B x D x E)  
(minimum distance from livestock barn)

272 m (892 ft)

Actual distance from livestock barn

NA

Storage base distance 'S'  
(minimum distance from manure storage)

No existing manure storage

Actual distance from manure storage

NA

[https://agrisuite.omafra.gov.on.ca/MINIMUM\\_DISTANCE\\_SEPARATION\\_1?worksheetId=f7295cdf-39c0-49fe-ae32-f27b272ce98e](https://agrisuite.omafra.gov.on.ca/MINIMUM_DISTANCE_SEPARATION_1?worksheetId=f7295cdf-39c0-49fe-ae32-f27b272ce98e)

2/6

Farm contact information

ON



Location of existing livestock facility or anaerobic digester

County of Grey

Township of West Grey

NORMANBY

Concession A WEST OF OWEN SOUND ROAD ,

Lot 75

Roll number: 4205010007025000000

Total lot size

10 ha

Livestock/manure summary

Manure Form	Type of livestock/manure	Existing maximum number	Existing maximum number (NU)	Estimated livestock barn area
Solid	Horses, Medium-framed, mature; 227 - 680 kg (including unweaned offspring)	13	13 NU	302 m²



Confirm Livestock/Manure Information (6884 On-89)

The livestock/manure information has not been confirmed with the property owner and/or farm operator.

Setback summary

Existing manure storage	V3. Solid, outside, no cover, >= 30% DM	
Design capacity	13 NU	
Potential design capacity	13 NU	
Factor A (odour potential)	0.7	Factor B (design capacity) 176.66
Factor D (manure type)	0.7	Factor E (encroaching land use) 2.2
Building base distance 'F' (A x B x D x E) (minimum distance from livestock barn)		191 m (627 ft)
Actual distance from livestock barn		NA
Storage base distance 'S' (minimum distance from manure storage)		191 m (627 ft)
Actual distance from manure storage		NA

Farm contact information ⓘ  
ON

Location of existing livestock facility or anaerobic digester  
County of Grey  
Township of West Grey  
NORMANBY  
Concession A WEST OF OWEN SOUND ROAD ,  
Lot 70  
Roll number: 4205010007027000000

Total lot size  
35 ha

Livestock/manure summary

Manure Form	Type of livestock/manure	Existing maximum number	Existing maximum number (NU)	Estimated livestock barn area
Liquid	Dairy, Heifers Large Frame (182 - 545 kg) (eg. Holsteins), Free Stall	266	133 NU	1853 m²

⚠ Confirm Livestock/Manure Information (42487 Grey County Rd 109)  
The livestock/manure information has not been confirmed with the property owner and/or farm operator.

Setback summary

Existing manure storage	M1. Liquid, outside, no cover, straight-walled storage		
Design capacity	133 NU		
Potential design capacity	133 NU		
Factor A (odour potential)	0.7	Factor B (design capacity)	348.89
Factor D (manure type)	0.8	Factor E (encroaching land use)	2.2
Building base distance 'F' (A x B x D x E) (minimum distance from livestock barn)			430 m (1411 ft)
Actual distance from livestock barn			NA
Storage base distance 'S' (minimum distance from manure storage)			487 m (1598 ft)
Actual distance from manure storage			NA

Farm contact information ⓘ  
ON

Location of existing livestock facility or anaerobic digester  
County of Grey  
Township of West Grey  
NORMANBY  
Concession A WEST OF OWEN SOUND ROAD ,  
Lot 72  
Roll number: 4205010007029000000

Total lot size  
30 ha

Livestock/manure summary

Manure Form	Type of livestock/manure	Existing maximum number	Existing maximum number (NU)	Estimated livestock barn area
Solid	Beef, Backgrounders (7 - 12.5 months), Confinement	29	9.7 NU	135 m²

⚠ Confirm Livestock/Manure Information (271087 County Rd 6)  
The livestock/manure information has not been confirmed with the property owner and/or farm operator.

Setback summary

Existing manure storage	No storage required (manure is stored for less than 14 days)		
Design capacity	9.7 NU		
Potential design capacity	9.7 NU		
Factor A (odour potential)	0.8	Factor B (design capacity)	165.56
Factor D (manure type)	0.7	Factor E (encroaching land use)	2.2
Building base distance 'F' (A x B x D x E) (minimum distance from livestock barn)			204 m (669 ft)
Actual distance from livestock barn			NA
Storage base distance 'S' (minimum distance from manure storage)			No existing manure storage
Actual distance from manure storage			NA

Preparer signoff & disclaimer

Preparer contact information  
ON

Signature of preparer

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Date (mmm-dd-yyyy)**Note to the user**

The Ontario Ministry of Agriculture, Food and Rural Affairs (OMAFRA) has developed this software program for distribution and use with the Minimum Distance Separation (MDS) Formulae as a public service to assist farmers, consultants, and the general public. This version of the software distributed by OMAFRA will be considered to be the official version for purposes of calculating MDS. OMAFRA is not responsible for errors due to inaccurate or incorrect data or information; mistakes in calculation; errors arising out of modification of the software, or errors arising out of incorrect inputting of data. All data and calculations should be verified before acting on them.

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# MDS I Setback Map



Prepared By:

Notes:

